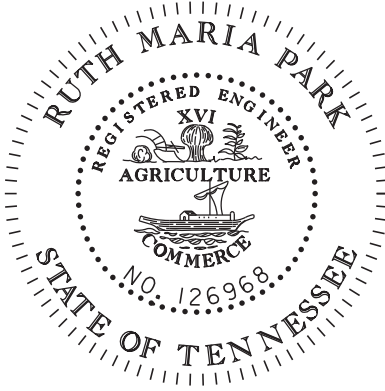




SHEET NAME	SHEET NO.
SIGNATURE SHEETS.....	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS.....	1A
PROJECT COMMITMENTS	1B
ESTIMATED ROADWAY AND TABULATED QUANTITIES.....	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B
GENERAL NOTES.....	2C
SPECIAL NOTES.....	2D
ENVIRONMENTAL NOTES.....	2E-2E1
PRESENT / PROPOSED & EPSC LAYOUT.....	3
TRAFFIC CONTROL PLANS	T1-T6
NOTE: THE ALPHABETICAL LETTERS "I", "O" & "Q" ARE NOT USED IN THE NUMBERING OF SHEETS.	

YEAR	PROJECT NO.	SHEET NO.
2025	19S045-M3-005	ROADWAY-SIGN
STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION		
SIGNATURE SHEET		



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

Ruth Park
2025.07.18 08:50:21 -05'00'

PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED. THE SIGNATURE MUST BE VERIFIED ON THE ELECTRONIC DOCUMENTS.

GRESHAM SMITH
222 2ND AVE SOUTH
NASHVILLE, TN 37201
RUTH PARK, P.E., P.E. NO. 126968

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	BRIDGE-SIGN1
LAYOUT OF BRIDGE TO BE REPAIRED	BR-132-842
ESTIMATED BRIDGE QUANTITIES.....	BR-132-843
BRIDGE GENERAL NOTES.....	BR-132-844
PHASE CONSTRUCTION DETAILS.....	BR-132-845
SUPERSTRUCTURE DETAILS	BR-132-846
MISCELLANEOUS REPAIR DETAILS.....	BR-132-847
ABUTMENT REPAIR DETAILS.....	BR-132-848
BILL OF STEEL	BR-132-849

YEAR	PROJECT NO.	SHEET NO.
2025	19S045-M3-005	BRIDGE - SIGN 1

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE
SHEET

Index Of Sheets
SEE SHEET NO. 1A

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

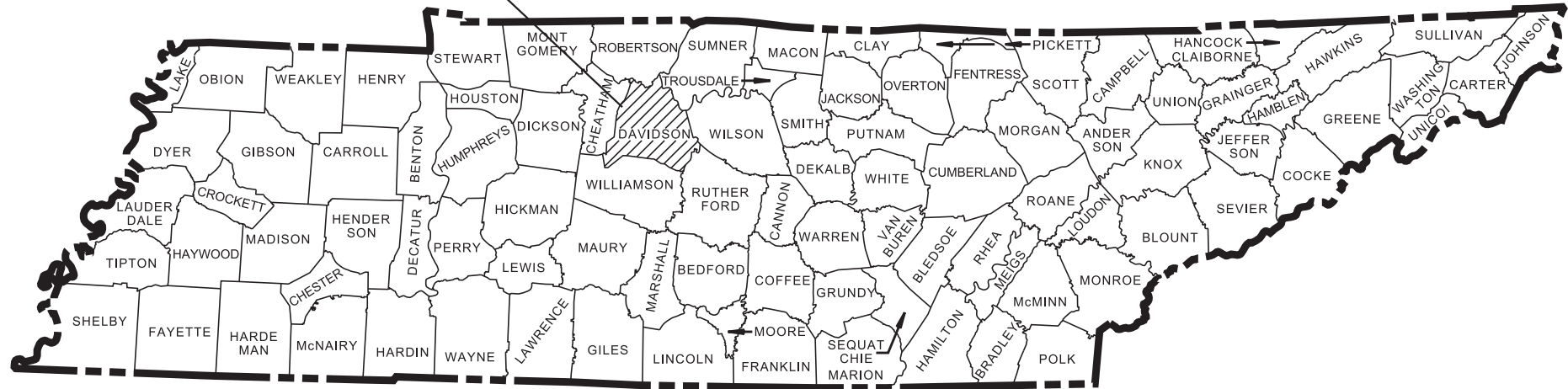
DAVIDSON COUNTY

OLD HICKORY BLVD.
S.R. 45 (LOG MILE 17.92)
OVER INTERSTATE 40

PS&E
BRIDGE REPAIR

PROJECT LOCATION

BRIDGE ID. # 19100400141



BEGIN PROJECT NO. 19S045-M3-005 PS&E

STA. 123+54.94

N 669348.8624 E 1790962.0331

END PROJECT NO. 19S045-M3-005 PS&E

STA. 127+36.05

N 668990.4713 E 1791090.0998

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT REPAIR OFFICE DESIGN MANAGER: JOSEPH BENDER, P.E.

DESIGNED BY: GRESHAM SMITH

DESIGNER: RUTH PARK, P.E. CHECKED BY DONALD MCCRARY, P.E.

P.E. NO. 19S045-M3-005

PIN NO. 134893.00



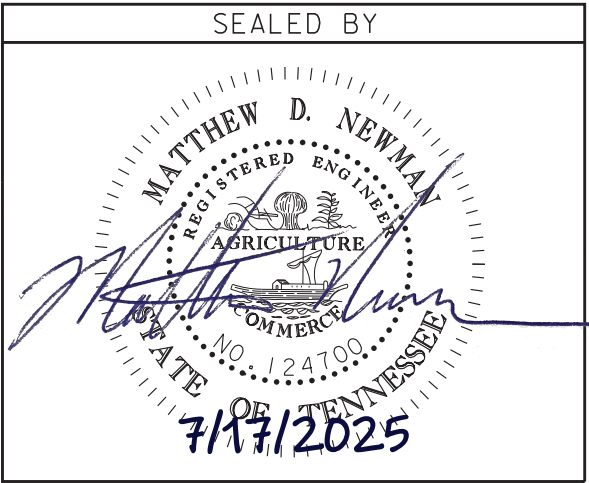
ROADWAY LENGTH 0.030 MILES
BRIDGE LENGTH 0.042 MILES
PROJECT LENGTH 0.072 MILES

- NO EXCLUSIONS
- LAND DISTURBANCE
LESS THAN 1 ACRE
- ROAD TO BE CLOSED
DURING CONSTRUCTION
- THIS PROJECT WILL
BE BUNDLED WITH
PIN 133533.00
AND PIN 113872.01

TRAFFIC DATA (SR-45 OLD HICKORY BLVD.)	
ADT (2024)	8,681
POSTED SPEED	40 MPH

TRAFFIC DATA (I-40)	
ADT (2024)	106,460
POSTED SPEED	70 MPH

TENN.	YEAR	SHEET NO.
	2025	1
FED. AID PROJ. NO.		
STATE PROJ. NO.	19S045-M3-005	



APPROVED: WILL REID, CHIEF ENGINEER

DATE:

APPROVED: WILL REID, COMMISSIONER

ROADWAY INDEX

SHEET NAME	SHEET NO.
SIGNATURE SHEETS.....	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS	1A
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ENVIRONMENTAL NOTES.....	2E-2E1
PRESENT / PROPOSED & EPSC LAYOUT.....	3
TRAFFIC CONTROL PLANS	T1-T6
BRIDGE PLANS	B-1
NOTE: THE ALPHABETICAL LETTERS “I”, “O” & “Q” ARE NOT USED IN THE NUMBERING OF SHEETS.	

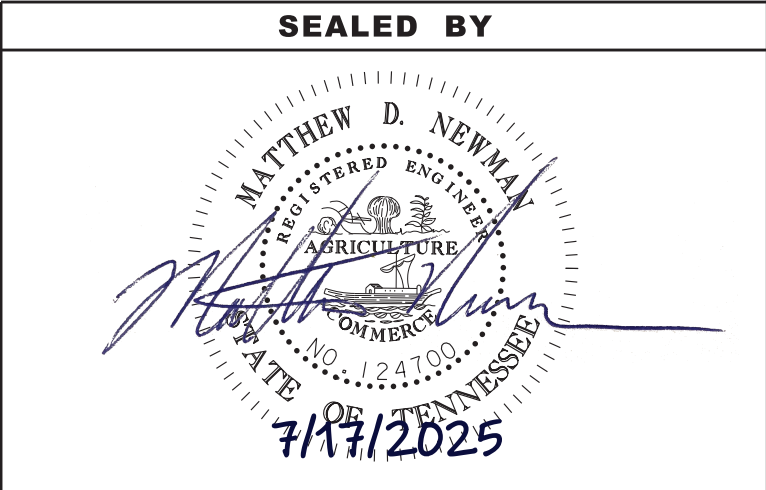
STANDARD ROADWAY DRAWINGS

DWG.	REV.	DESCRIPTION
10-100.00 STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS		
RD-TP-1	10-01-24	STANDARD ROADWAY DRAWINGS TITLE SHEET
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND
RD-L-5	07-30-24	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD-L-6	02-20-20	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD-L-7	02-20-20	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
10-101.00 ROADWAY DESIGN STANDARD		
RD11-TS-3B		DESIGN STANDARDS FOR ARTERIAL HIGHWAYS WITH FLUSH MEDIAN (4 AND 6 LANE)
RD11-TS-5B		DESIGN STANDARDS FOR FREEWAYS WITH MEDIAN BARRIER (4 AND 6 LANE)
10-104.00 ROADWAY, PAVEMENT APPURTENANCES, AND FENCES		
RP-VC-10	03-04-21	VERTICAL CONCRETE CURB AND CURB AND GUTTER (FOR 8” TO 12” GUTTER DEPTH)
10-105.00 MULTIMODAL		
MM-SW-1	07-07-23	DETAILS FOR CONCRETE SIDEWALKS
10-106.00 SAFETY DESIGN AND GUARDRAILS		
S-GR31-1	06-15-21	GUARDRAIL DETAILS
S-GR31-1A	06-28-19	GUARDRAIL AND BLOCK-OUT DETAILS
S-GR31-1B		GUARDRAIL FASTENING HARDWARE
S-GR31-1C	07-07-23	GUARDRAIL GENERAL NOTES AND POST DETAILS
S-GR31-1D	03-01-23	GUARDRAIL POST PLACEMENT IN ROCK
S-GRC-4	01-30-25	GUARDRAIL CONNECTION TO BRIDGE RAILING CONCRETE PARAPET
10-107.00 EROSION PREVENTION AND SEDIMENT CONTROL		
EC-STR-8	06-10-14	FILTER SOCK
EC-STR-37	06-10-14	SEDIMENT TUBE

STANDARD TRAFFIC DESIGN DRAWINGS

DWG.	REV.	DESCRIPTION
10-204.00 DESIGN - TRAFFIC CONTROL		
T-M-1	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-3	01-24-25	MARKING STANDARDS FOR TRAFFIC ISLANDS, PAVED SHOULDERS AND MEDIANS FOR CONVENTIONAL ROADS
T-M-4	01-24-25	STANDARD INTERSECTION PAVEMENT MARKINGS
T-M-4A	01-24-25	STANDARD UNSIGNALIZED MID-BLOCK CROSSING
T-WZ-16	07-30-24	LANE SHIFT FOR DIVIDED HIGHWAYS AND FREEWAYS
T-WZ-FAB1		FLASHING YELLOW ARROW BOARD
T-WZ-PCB1	10-10-24	10 FOOT PORTABLE CONCRETE BARRIER RAIL
T-WZ-PCB2	10-01-24	20 FOOT PORTABLE CONCRETE BARRIER RAIL
T-WZ-PCB2A	10-01-24	20 FOOT PORTABLE CONCRETE BARRIER RAIL STIFFENER TUBE
T-WZ-PCB3	01-28-22	PORTABLE CONCRETE BARRIER RAIL DETAILS
T-WZ-PCB4	12-09-22	PORTABLE CONCRETE BARRIER RAIL ANCHOR PIN DETAILS

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	19S045-M3-005	1A




STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ROADWAY INDEX
AND
STANDARD
ROADWAY
DRAWINGS

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	19S045-M3-005	1B

PROJECT COMMITMENTS			
COMMITMENT ID	SOURCE DIVISON	DESCRIPTION	STA. / LOCATION
EDHZ001	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	AN ASBESTOS CONTAINING MATERIAL (ACM) SURVEY WAS COMPLTETED ON BRIDGE NO.19-I00400141 OVER INTERSTATE 40 (19-SR045-17.92). NO ASBESTOS WAS DETECTED. PLEASE SEE THE REPORT FOR FURTHER DETAILS AND PHOTOGRAPHS. NO SPECIAL ACCOMMODATIONS FOR DEMOLITION AND WASTE DISPOSAL ARE ANTICIPATED FOR THESE STRUCTURES AND THE MATERIAL CAN BE DEPOSITED IN A C&D LANDFILL. PRIOR TO THE DEMOLITION OR REHABILITATION OF ANY STRUCTURE (BRIDGE OR BUILDING), THE CONTRACTOR IS REQUIRED TO SUBMIT THE NATIONAL EMISSION STANDARDS FOR HAZARDOUS AIR POLLUTANTS STANDARD 10 DAY NOTICE OF DEMOLITION TO THE TDEC DIVISION OF AIR POLLUTION CONTROL (STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (JANUARY 1, 2021) SECTIONS 107.08D AND 202.03).	BRIDGE NO.19-I00400141
ETR3001	ENVIRONMENTAL TECH OFFICE, REGION 3	IN ACCORDANCE WITH THE PROGRAMMATIC CONSULTATION FOR ADDRESSING CLIFF SWALLOWS AND BARN SWALLOWS ON TRANSPORTATION PROJECTS DATED 9/16/2020, CLIFF SWALLOW AND BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) WILL NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS MAY BE REMOVED AND DESTROYED AND MESAURES MAY BE IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (E.G., CLOSING OFF AREA USING NETTING).	BRIDGE NO.19-I00400141

SEALED BY



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PROJECT
COMMITMENTS

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	19S045-M3-005	2

ESTIMATED ROADWAY QUANTITIES

[illegible]

FOOTNOTES:

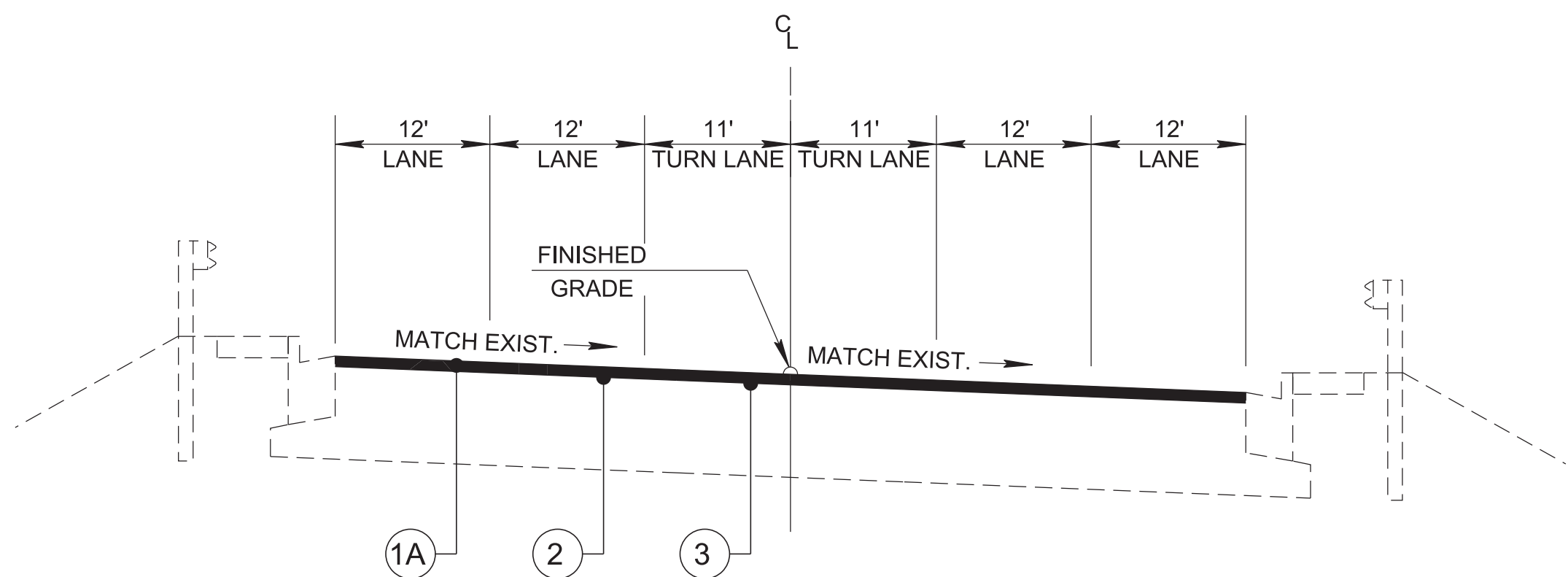
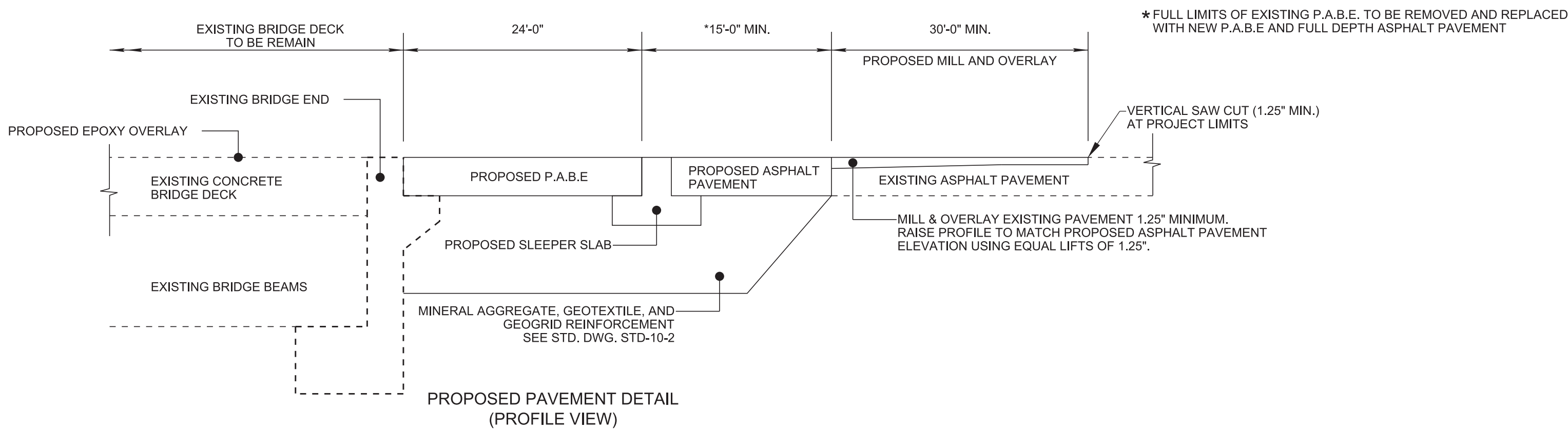
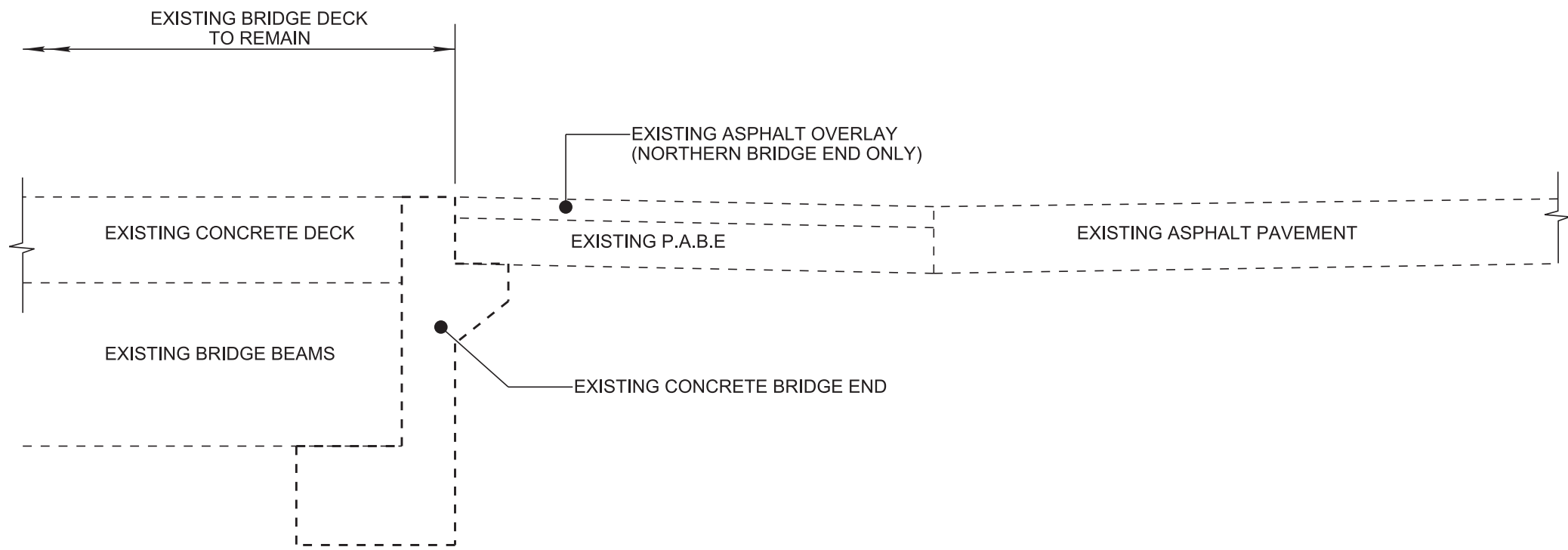
- (1) TO BE USED AS DIRECTED BY ENGINEER.
- (2) SEE SUBSECTION 209.07 OF THE STANDARD SPECIFICATIONS FOR MAINTENANCE REPLACEMENT
- (3) INCLUDES ANY GUARDRAIL REMOVAL REQUIRED FOR CONSTRUCTION AND THE REPLACEMENT OF EXISTING GUARDRAIL AFTER CONSTRUCTION.
- (4) THIS ITEM SHALL BE A PORTABLE ENERGY ABSORBING TERMINAL MEETING THE REQUIREMENTS OF AASHTO MASH FOR TEST LEVEL 3. THE PAY ITEM WILL INCLUDE FURNISHING AND INSTALLING ALL COMPONENTS AS SHOWN ON THE MANUFACTURER'S DRAWING.
- (5) SEE SHEET T2 FOR TRAFFIC CONTROL SIGN SCHEDULE, ADDITIONAL SIGNS INSTALLED AS DIRECTED BY THE ENGINEER SHALL BE PAID FOR AT THE UNIT COST FOR THIS ITEM.
- (6) NO BARRIER RAIL SHALL BE PLACED EARLIER THAN 10 DAYS BEFORE WORK IS TO BE DONE AT THAT LOCATION. IF NO WORK TAKES PLACE IN THE CONSTRUCTION ZONE BEHIND BARRIER RAIL OVER 2 SUCCESSIVE WEEKS AND THE ENGINEERS DETERMINE THAT THE RAIL IS NOT WARRANTED, THE BARRIER RAIL MUST BE REMOVED AT THE CONTRACTORS EXPENSE AND A SAFE CONDITION CREATED.
- (7) ALL ARROW BOARDS ALONG OLD HICKORY BLVD. MAY BE TYPE B 60" X 30".
- (8) AGGREGATE SIZE MUST BE ONE OF THE APPROVED OPTIONS PROVIDED IN STANDARD DRAWING STD-10-2
- (9) ITEM TO BE USED FOR PERMANENT PAVEMENT MARKING PROPOSED ON CONCRETE OR EPOXY OVERLAYED SURFACES.
- (10) ITEM TO BE USED TO REPLACE PERMANENT PAVEMENT MARKINGS THAT CONFLICTED WITH TEMPORARY TRAFFIC CONTROL PAVEMENT MARKING LINES.
- (11) RIBBON METHOD OF APPLICATION WILL NOT BE PERMITTED ON CONCRETE OR NEW EPOXY COATED ROADWAY SURFACES.
- (12) INCLUDES THE COST OF ANY TEMPORARY ADJUSTMENTS OF SIGNAL HEAD LOCATIONS AS DIRECTED BY THE ENGINEER DURING PHASED CONSTRUCTION

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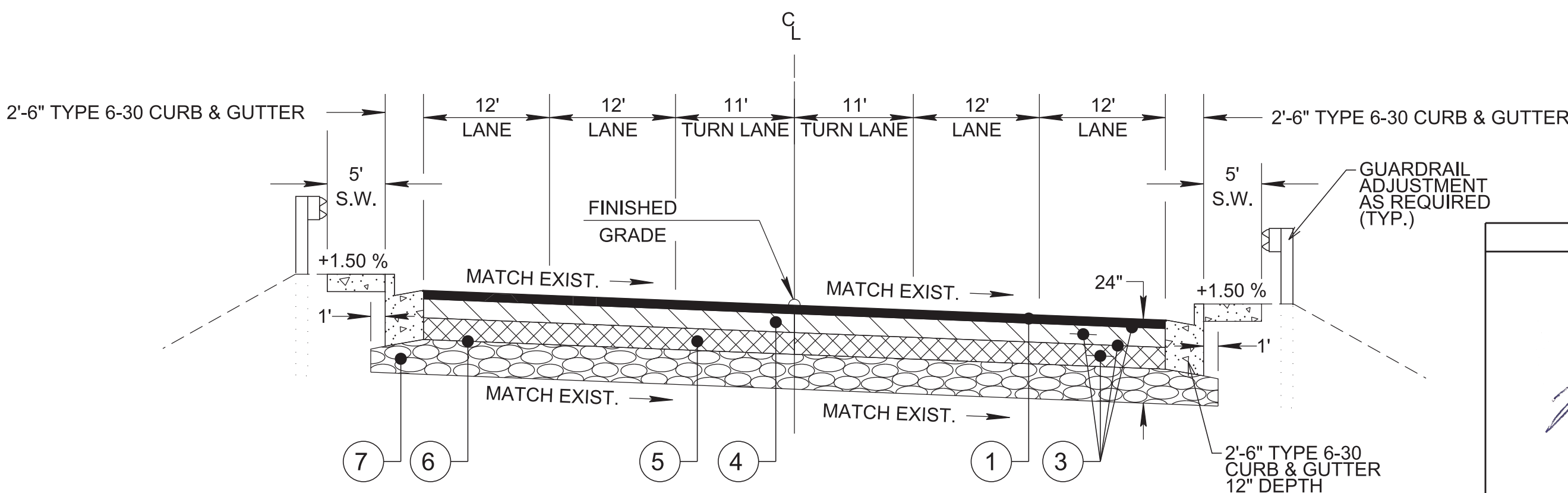
**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

ESTIMATED QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	19S045-M3-005	2B



SUPERELEVATED SECTION
OLD HICKORY BLVD.
MILL AND OVERLAY



SUPERELEVATED SECTION
OLD HICKORY BLVD.
FULL DEPTH PAVEMENT

PROPOSED PAVEMENT SCHEDULE

① ASPHALTIC CONCRETE SURFACE (HOT MIX) PG70-22
GRADING "D" SURFACE @ 1.25" THICK (APPROX. 132.5 LB./S.Y.)

411-02.10 ACS MIX (PG70-22) GRADING "D"

①A ASPHALTIC CONCRETE SURFACE (HOT MIX) PG70-22
GRADING "D" SURFACE @ VARIABLE THICKNESS
(APPLIED IN EQUAL LIFTS OF 1.25")
411-02.10 ACS MIX (PG70-22) GRADING "D"

② COLD PLANING BITUMINOUS PAVEMENT
MILLING @ 1.25" THICK
415-01.02 COLD-PLANING BITUMINOUS PAVEMENT

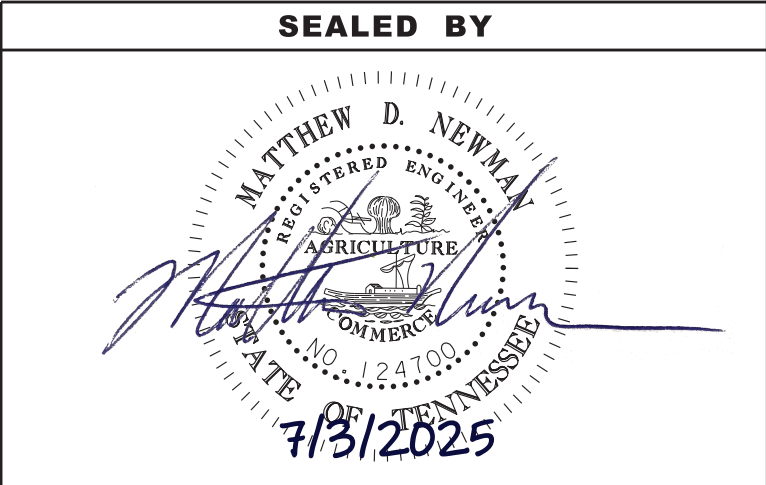
③ TACK COAT
403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC)
SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD

④ BITUMINOUS PLANT MIX BASE (HOT MIX) PG70-22
GRADING "B-M2" @ 4.50" THICK (APPROX. 508.5 LB./S.Y.)
(APPLIED IN TWO EQUAL LIFTS OF 2.25")
307-02.08 ASPHALT CONCRETE MIX (PG70-22) (BPMB-HM) GRADING "B-M2"

⑤ BITUMINOUS PLANT MIX BASE (HOT MIX) PG70-22
GRADING "A" @ 6.25" THICK (APPROX. 718.75 LB./S.Y.)
(APPLIED IN LIFTS OF 3" and 3.25")
307-02.01 ASPHALT CONCRETE MIX (PG70-22) (BPMB-HM) GRADING "A"

⑥ PRIME COAT
402-01 BITUMINOUS MATERIAL FOR PRIME COAT (PC) AT 0.35 GALLONS/S.Y.
402-02 AGGREGATE FOR COVER MATERIAL (PC) AT 12 LB./S.Y.

⑦ MINERAL AGGREGATE 12" THICK
303-01 MINERAL AGGREGATE, TYPE "A" BASE, GRADING "D"



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL
SECTIONS AND
PAVEMENT
SCHEDULE

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GENERAL NOTES

GRADING

- (1)

ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (2)

CERTIFICATION FOR ALL BORROW PITS MUST BE OBTAINED IN ACCORDANCE WITH SUBSECTION 107.06 OF THE STANDARD SPECIFICATIONS.
- (3)

THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

GUARDRAIL

- (1)

THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPES UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.
- (3)

IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE "A" LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING TEMPORARY BARRICADES OR DRUMS WITH TYPE "A" LIGHTS TO DELINEATE GUARDRAIL END AND A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL END TERMINAL.
- (4)

GUARDRAIL IS TO BE COMPLETE IN PLACE BEFORE THE MAINLINE ROADWAY IS OPENED TO TRAFFIC.

MISCELLANEOUS

- (3)

NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

ROAD CLOSURE

- (1)

NO LESS THAN SEVEN (7) DAYS PRIOR TO THE CLOSURE OF THE ROAD, THE CONTRACTOR SHALL NOTIFY THE FOLLOWING INDIVIDUALS OR AGENCIES COMPLETELY DESCRIBING THE AFFECTED ROADS AND THE APPROXIMATE DURATION OF THE CONSTRUCTION: THESE PARTIES INCLUDE, BUT ARE NOT LIMITED TO: (1) LOCAL LAW ENFORCEMENT OFFICE, (2) LOCAL FIRE DEPARTMENT, (3) AMBULANCE SERVICE, (4) LOCAL SCHOOL SUPERINTENDENT, (5) UNITED STATES POSTAL SERVICE, AND (6) LOCAL ROAD SUPERINTENDENT.

PAVEMENT MARKINGS

FINAL PAVEMENT MARKING

- (8)

PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. **716-12.02**, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

DETOURS, LANE SHIFTS AND MEDIAN CROSS-OVERS

- (16)

THE PAVEMENT MARKING ON THE LANE SHIFTS FOR OLD HICKORY BLVD TRAVEL LAENS WILL BE INSTALLED AND MAINTAINED TO THE SAME STANDARDS AS FOR PERMANENT MARKINGS ON THE MAIN ROADWAY.

THESE MARKINGS SHALL BE IN PLACE PRIOR TO ALLOWING TRAFFIC ONTO THE PAVEMENT. THESE PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO.716-05.02, L.F.

- (17)

BEFORE OPENING THE LANE SHIFTS TO TRAFFIC, THE TRANSITIONAL MARKINGS ON THE EXISTING ROADWAY MUST BE IN PLACE. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.02 PER L.F. ALL EXISTING MARKINGS IN THE AREA OF THESE TRANSITIONAL MARKINGS SHALL BE OBLITERATED AND ALL EXISTING RAISED PAVEMENT MARKERS SHALL BE REMOVED TO ELIMINATE CONFLICTING MARKINGS. REMOVAL OF THE EXISTING CONFLICTING MARKINGS AND RAISED PAVEMENT MARKERS WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COST WILL BE INCLUDED IN ITEM NO. **712-01**, TRAFFIC CONTROL, LUMP SUM.
- (18)

BEFORE OPENING THE LANE SHIFTS TO TRAFFIC, THE TRANSITIONAL MARKINGS ON THE EXISTING ROADWAY MUST BE IN PLACE. ALL EXISTING MARKINGS IN THE AREA OF THESE TRANSITIONAL MARKINGS SHALL BE OBLITERATED AND ALL EXISTING RAISED PAVEMENT MARKERS SHALL BE REMOVED TO ELIMINATE CONFLICTING MARKINGS. REMOVAL OF THE EXISTING CONFLICTING MARKINGS AND RAISED PAVEMENT MARKERS WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COST WILL BE INCLUDED IN ITEM NO. **712-01**, TRAFFIC CONTROL, LUMP SUM.

PAVEMENT

PAVING

- (1)

THE CONTRACTOR SHALL BE REQUIRED TO PAVE IN THE DIRECTION OF TRAFFIC.
- (2)

THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (4)

WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (5)

ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (7)

ON CURB AND GUTTER SECTIONS, PUBLIC ROAD INTERSECTIONS SHALL BE RESURFACED TO THE END OF RADIUS. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD SHALL BE PROVIDED.
- (8)

ON URBAN TYPICAL SECTIONS, (CURB AND GUTTER), RESIDENTIAL DRIVEWAYS AND BUSINESS ENTRANCES SHALL HAVE A MINIMUM WIDTH OF MATERIAL NOT LESS THAN ONE FOOT USED IN THE TRANSITION TO FEATHER THE PAVEMENT EDGE.
- (9)

IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1)

ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2)

IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. **712-06**, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3)

A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4)

TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5)

USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY

BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

- (6)

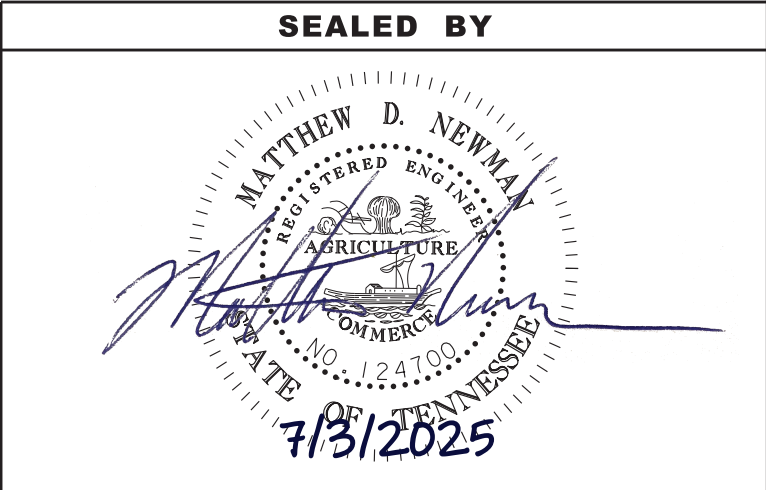
THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (7)

ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (8)

ALL DETOURS SHALL BE PAVED, STRIPED, SIGNED, AND FLEXIBLE DRUMS ARE TO BE IN PLACE BEFORE IT IS OPENED TO TRAFFIC.
- (9)

THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. **712-06**, SIGNS (CONSTRUCTION), S.F.

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STATE OF TENNESSEE
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GENERAL
NOTES

SPECIAL NOTES

DEMOLITION

DEMOLITION, REPAIR, OR REHABILITATION OF BRIDGES

- (1) THE CONTRACTOR SHALL VERIFY THAT AN ASBESTOS SURVEY HAS BEEN COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATIONS ACTIVITIES (NOT INCLUDING ASPHALT MILLING OR OVERLAY).
- (2) ASBESTOS-CONTAINING MATERIALS (ACM) ABATEMENT IS THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATION OF BRIDGE(S). ABATEMENT SHOULD BE ACCOMPLISHED PER SP202ACM SPECIAL PROVISION REGARDING REMOVAL OF ASBESTOS-CONTAINING MATERIALS. STATE OF TENNESSEE ASBESTOS ACCREDITATION REQUIREMENTS (TCA 1200-01-20) MANDATE THAT ACM ABATEMENT WORK BE PERFORMED BY AN ACCREDITED FIRM (CONTRACTOR) USING ACCREDITED ABATEMENT WORKERS AND SUPERVISORS.
- (3) THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUBMITTING A NOTICE TO THE TDEC, DIVISION OF AIR POLLUTION CONTROL TEN (10) DAYS IN ADVANCE OF ANY ACM ABATEMENT, DEMOLITION, OR MAJOR REPAIR INVOLVING THE REMOVAL/REPLACEMENT OF A STRUCTURAL COMPONENT.

PAVEMENT

RESURFACING

- (1) TRAFFIC WILL BE ALLOWED TO TEMPORARILY DRIVE ON THE MILLED SURFACE OF THE ROADWAY UNDER THE FOLLOWING CONDITIONS ONLY:

A. THE MILLED SURFACE IS FINE TEXTURED. THE FINE TEXTURE SHALL BE OBTAINED BY A MILLING MACHINE UTILIZING A MILLING HEAD WITH TEETH SPACING 3/8" OR LESS OPERATING AT LESS THAN 80 FEET PER MINUTE.

B. THE SURFACE SHALL BE SWEEPED AND CLEANED OF ALL LOOSE MATERIALS.

C. THE MILLED SURFACE SHALL BE PAVED WITHIN 72 HOURS IF THE CURRENT ADT IS ≥ 70,000 OR WITHIN 96 HOURS IF THE CURRENT ADT IS < 70,000.

D. RAIN OR INCLEMENT WEATHER IS NOT EXPECTED OR FORECASTED WITHIN 48 HOURS AFTER MILLING.

E. ALL APPLICABLE SIGNING IS INSTALLED IN ACCORDANCE WITH THE MUTCD. SIGNING SHALL INCLUDE MOTORCYCLE WARNING SIGNS (TN-64) PLACED IN ADVANCE OF ANY MILLED AREAS

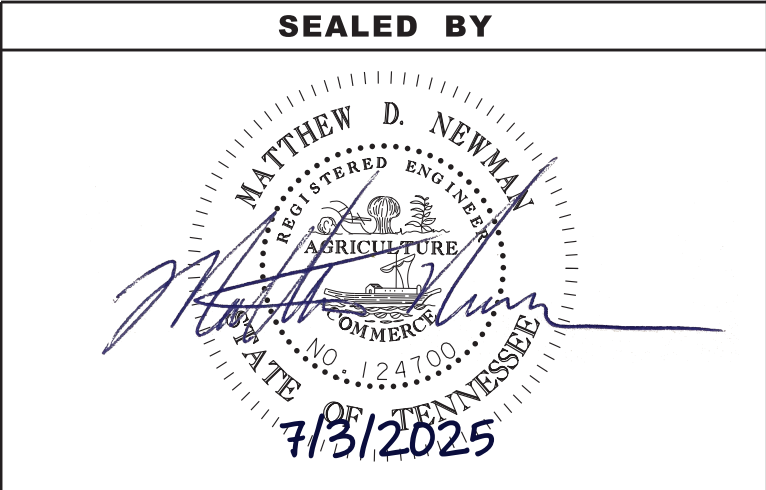
F. IF MILLED SURFACE BEGINS TO DETERIORATE, PAVING TO COVER UP DETERIORATING MILLED SURFACES SHOULD OCCUR AS DIRECTED BY THE ENGINEER DURING THE NEXT WORKING DAY. IF SEVERE DISTRESS OCCURS, IMMEDIATE RESPONSE WILL BE REQUIRED.

G. ONLY ONE LANE IN EACH DIRECTION SHALL HAVE A MILLED SURFACE AT ONE TIME.

EXISTING FACILITIES

- (1) IF ANY EXISTING FACILITIES ARE DAMAGED, IT IS THE RESPONSIBILITY AND AT THE COST OF THE CONTRACTOR TO REPLACE/REPAIR THESE FACILITIES.

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SPECIAL
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ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

SUPPORT ACTIVITIES

- (18) MATERIALS AND STAGING AREAS SHALL NOT AFFECT ANY WATERS OF THE STATE/U.S. UNLESS THESE AREAS ARE SPECIFICALLY COVERED BY ENVIRONMENTAL PERMITS, OBTAINED SOLELY BY THE CONTRACTOR. THE CONTRACTOR SHALL REVIEW ALL EXISTING PERMITS TO ENSURE THAT WORK AT PERMITTED SITES DOES NOT EXCEED EXPIRATION DATES. IF WORK IS GOING TO BE CONTINUED AFTER EXPIRATION DATES, THE CONTRACTOR SHALL CONTACT THE TDOT PROJECT RESPONSIBLE PARTY TO COMMENCE PERMIT RENEWAL PROCESS.

ENVIRONMENTAL

- (20) EXCEPT AS OTHERWISE SPECIFIED, THERE ARE NO KNOWN SPECIAL ENVIRONMENTAL FACTORS PRESENT ON THIS PROJECT THAT INDICATE A NEED FOR SEASONAL LIMITATIONS ON THE CLEARING, GRUBBING, EXCAVATION, GRADING, CUTTING OR FILLING OPERATIONS OR ON THE TOTAL AREA OF EXPOSED SOIL.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

ECOLOGY

- (2) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.

- (4) ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

PROJECT COMMITMENTS


- (5) SEE PROJECT COMMITMENTS, SHEET 1B, FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.

SCOPE OF WORK

- (6) BRIDGE REPAIRS, P.A.B.E REPLACEMENT, & THIN EPOXY OVERLAY

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ENVIRONMENTAL
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ENVIRONMENTAL NOTES

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

SEDIMENT CONTROL

- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.

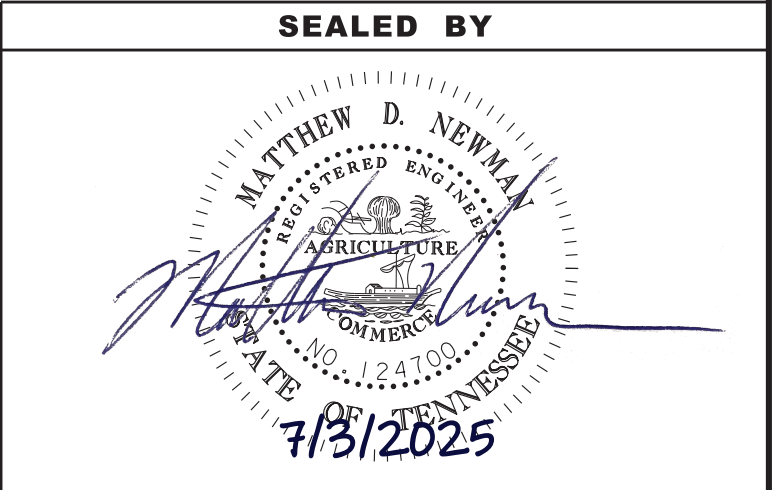
- (35) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.

- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

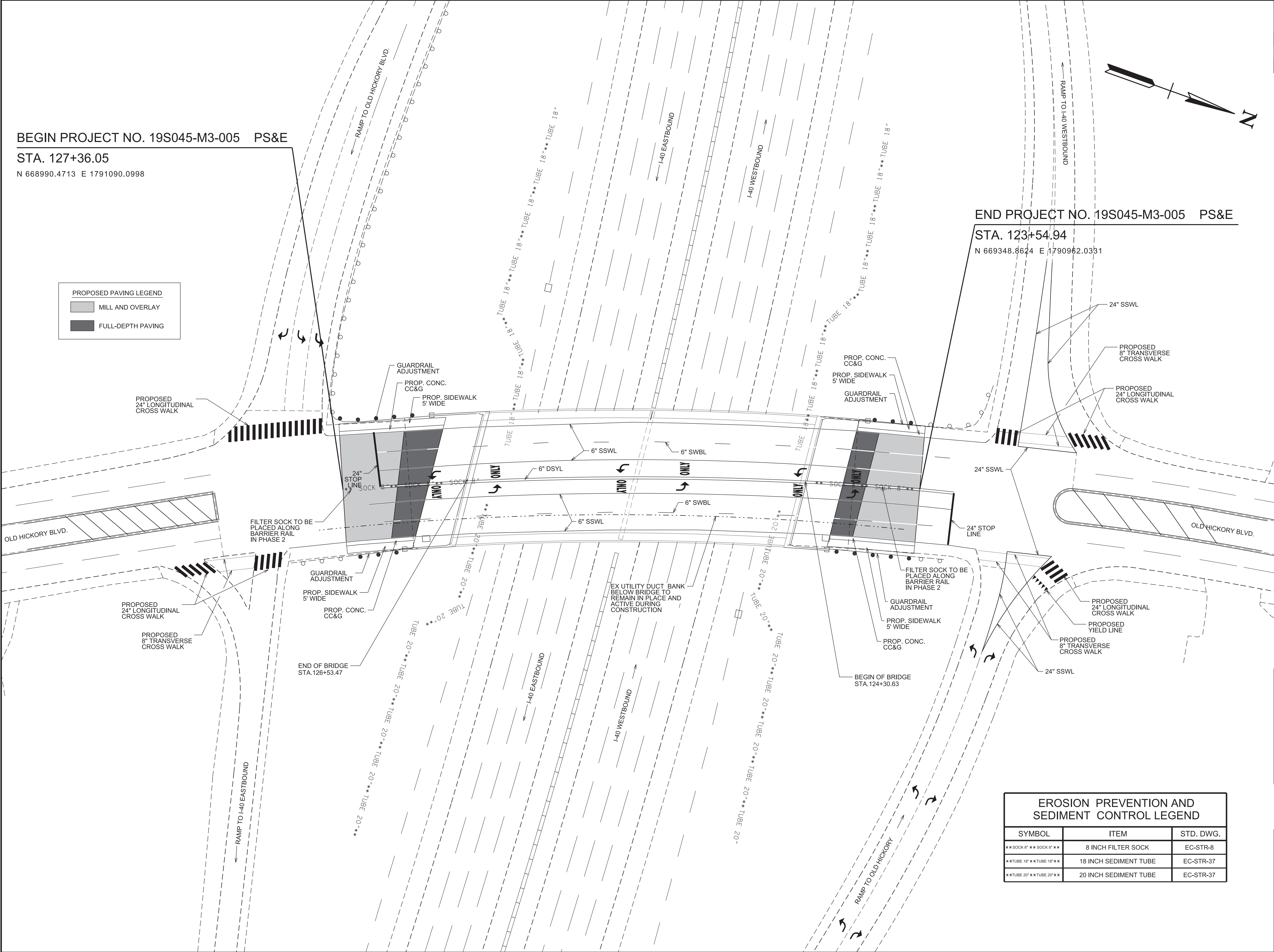
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PS&E	2025	19S045-M3-005	3

BEGIN PROJECT NO. 19S045-M3-005 PS&E
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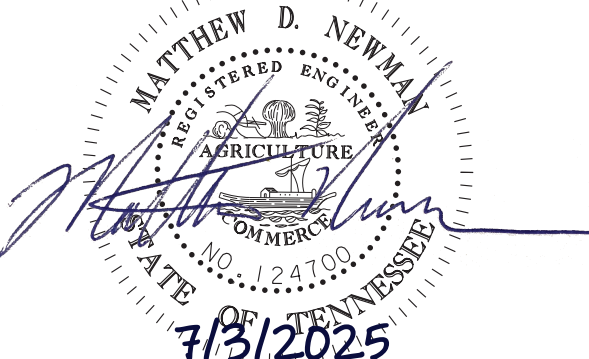
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STA. 123+54.94
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PROPOSED PAVING LEGEND	
	MILL AND OVERLAY
	FULL-DEPTH PAVING



EROSION PREVENTION AND SEDIMENT CONTROL LEGEND		
SYMBOL	ITEM	STD. DWG.
*** SOCK 6" *** SOCK 6" ***	8 INCH FILTER SOCK	EC-STR-8
** TUBE 18" ** TUBE 18" **	18 INCH SEDIMENT TUBE	EC-STR-37
* TUBE 20" * TUBE 20" *	20 INCH SEDIMENT TUBE	EC-STR-37

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7/3/2025

STATE OF TENNESSEE
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PRESENT/PROPOSED
& EPSC LAYOUT

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PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:

a. WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
2. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.

a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b. IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

c. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE REGIONAL TRAFFIC ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

3. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER'S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:

a. THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
- IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.
- b. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.

c. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO SIX INCHES OR LESS.

d. THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.
- FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN WITH PLAQUE (W8-17 AND W8-17P). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.
4. FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES.
- SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.
- IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.
- B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:
1. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 3/4 INCH AND NOT EXCEEDING 2 INCHES.

a. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

2. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:

a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

3. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:

a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b. ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:


1. WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

2. WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	19S045-M3-005	T1

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT EDGE
DROP-OFF NOTES
FOR
TRAFFIC CONTROL

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TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	19S045-M3-005	T2

TRAFFIC CONTROL QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
(6) 712-01	TRAFFIC CONTROL	LS	0.33
(4) 712-02.10	PORTABLE BARRIER RAIL (MASH TL-3)	L.F.	140
(4) 712-02.12	PORTABLE BARRIER RAIL, REDUCED DEFLECTION (MASH TL-3)	L.F.	656
(2) 712-02.60	TEMPORARY CRASH CUSHION (MASH TL-3)	EACH	2
712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	128
712-04.50	BARRIER RAIL DELINEATOR	EACH	2
712-05.01	WARNING LIGHTS (TYPE A)	EACH	8
(3) 712-06	SIGNS (CONSTRUCTION)	S.F.	437
712-07.02	TEMPORARY BARRICADES (TYPE II)	L.F.	72
712-07.03	TEMPORARY BARRICADES (TYPE III)	L.F.	96
(5) 712-08.03	ARROW BOARD (TYPE B)	EACH	6
(1) 713-16.01	CHANGEABLE MESSAGE SIGN UNIT	EACH	8
716-05.05	PAINTED PAVEMENT MARKING (STOP LINE)	L.F.	142
716-05.20	PAINTED PAVEMENT MARKING (6" LINE)	L.M.	1.8

FOOTNOTES:

- (1) TO BE USED AS DIRECTED BY ENGINEER.
- (2) THIS ITEM SHALL BE A PORTABLE ENERGY ABSORBING TERMINAL MEETING THE REQUIREMENTS OF AASHTO MASH FOR TEST LEVEL 3. THE PAY ITEM WILL INCLUDE FURNISHING AND INSTALLING ALL COMPONENTS AS SHOWN ON THE MANUFACTURER'S DRAWING.
- (3) SEE SHEET T2 FOR TRAFFIC CONTROL SIGN SCHEDULE, ADDITIONAL SIGNS INSTALLED AS DIRECTED BY THE ENGINEER SHALL BE PAID FOR AT THE UNIT COST FOR THIS ITEM.
- (4) NO BARRIER RAIL SHALL BE PLACED EARLIER THAN 10 DAYS BEFORE WORK IS TO BE DONE AT THAT LOCATION. IF NO WORK TAKES PLACE IN THE CONSTURCTION ZONE BEHIND BARRIER RAIL OVER 2 SUCESSIVE WEEKS AND THE ENGINEERS DETERMINE THAT THE RAIL IS NOT WARRANTED, THE BARRIER RAIL MUST BE REMOVED AT THE CONTRACTORS EXPENSE AND A SAFE CONDITION CREATED.
- (5) ALL ARROW BOARDS ALONG OLD HICKORY BLVD. MAY BE TYPE B 60" X 30".
- (6) INCLUDES THE COST OF ANY TEMPORARY ADJUSTMENTS OF SIGNAL HEAD LOCATIONS AS DIRECTED BY THE ENGINEER DURING PHASED CONSTRUCTION

TRAFFIC CONTROL SIGNS				
M.U.T.C.D. NO.	DESCRIPTION	SIZE	NO.	ITEM NO. 712-06 (S.F.)
G20-2	END ROAD WORK	48" X 24"	2	16
M1-1.2	I-40	24" X 24"	4	16
M3-2	EAST	24" X 12"	2	4
M3-4	WEST	24" X 12"	2	4
M6-1	RIGHT ARROW	21" X 15"	4	9
R3-7R	RIGHT LANE MUST TURN RIGHT	36" X 36"	8	72
R3-7R (MOD)	TRUCKS MUST TURN RIGHT	36" X 36"	4	36
R9-9	SIDEWALK CLOSED	24" X 12"	8	16
R11-2	ROAD CLOSED	48" X 30"	4	40
W1-4R	REVERSE CURVE (RIGHT)	48" X 48"	2	32
W1-4L	REVERSE CURVE (LEFT)	48" X 48"	2	32
W4-3L	ADDED LANE	48" X 48"	2	32
W20-1	ROAD WORK AHEAD	48" X 48"	8	128
			TOTAL	437

PHASING NOTES

PHASE 1 - TRAFFIC CONTROL

- *WESTERN HALF OF OLD HICKORY BLVD BRIDGE AND P.A.B.E. CLOSED FOR REPAIRS.
- *SB AND NB TRAFFIC SHIFTED TO THE EAST SIDE OF THE OLD HIKCORY BLVD BRIDGE.
- *OLD HICKORY BLVD LANES REDUCED TO 1 NB / 1 SB / 1 MEDIAN TURN LANE (ALL 10' WIDE) ON BRIDGE.
- *DETOURS REQUIRED FOR WB I-40 TO SB OLD HICKORY BLVD AND EB I-40 TO NB OLD HICKORY BLVD (LARGE TRUCKS ONLY)


PHASE 2 - TRAFFIC CONTROL

- *EASTERN HALF OF OLD HICKORY BLVD BRIDGE AND P.A.B.E CLOSED FOR REPAIRS.
- *SB AND NB TRAFFIC SHIFTED TO THE WEST SIDE OF THE OLD HIKCORY BLVD BRIDGE.
- *OLD HICKORY BLVD LANES REDUCED TO 1 NB / 1 SB / 1 MEDIAN TURN LANE (ALL 10' WIDE) ON BRIDGE.
- *DETOURS REQUIRED FOR WB I-40 TO SB OLD HICKORY BLVD AND EB I-40 TO NB OLD HICKORY BLVD (LARGE TRUCKS ONLY)

GENERAL NOTES

- *CONTRACT SHALL COORDINATE ACCESS WITH ADJACENT BUSINESS TO ENSURE AND MAINTAIN SUFFICIENT INGRESS AND EGRESS TO IMPACTED PROPERTIES DURING CONSTRUCTION.

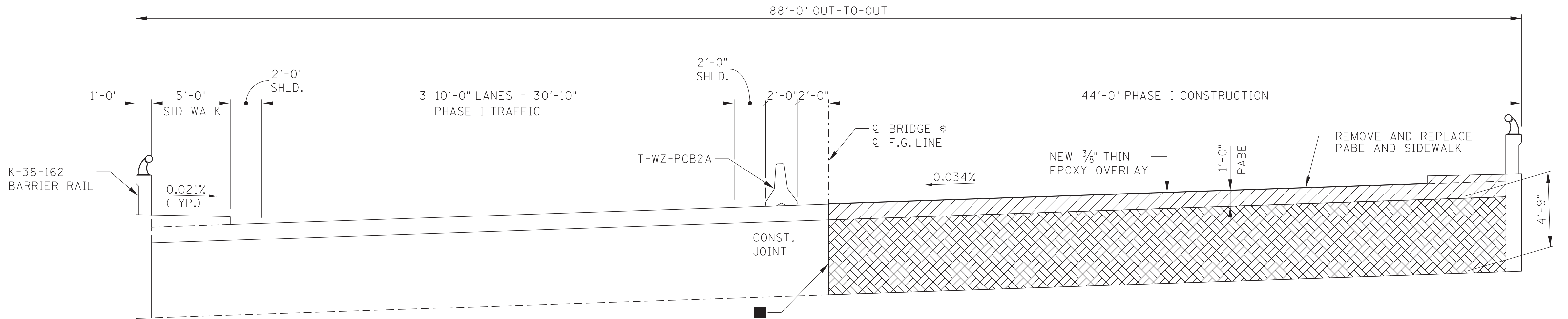
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TABULATED
TRAFFIC
CONTROL
QUANTITIES

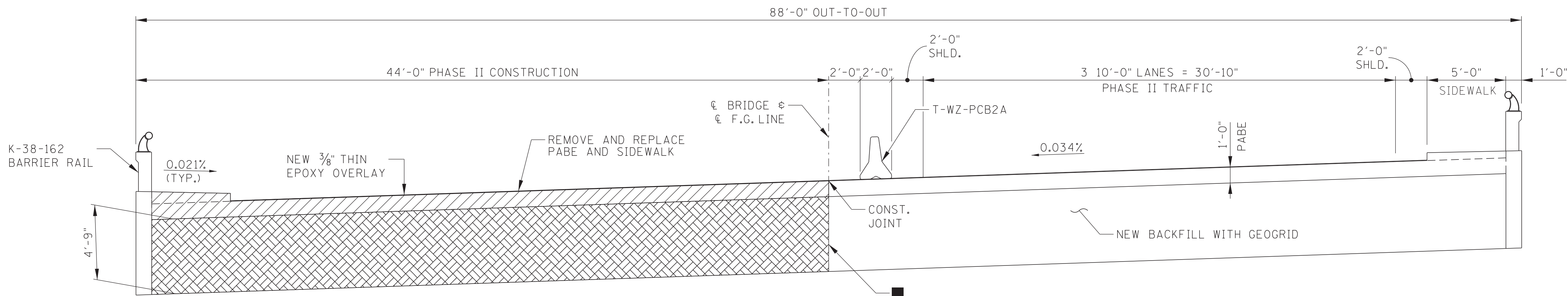
TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	19S045-M3-005	T3



■ DENOTES: TEMPORARY SHORING MAY BE NECESSARY FOR EXCAVATION ADJACENT TO TRAFFIC LANES. THE COST OF DESIGN, INSTALLATION AND REMOVAL OF ANY REQUIRED SHORING SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 604-03.04, PAVEMENT AT BRIDGE ENDS, S.Y.

PHASE I - CONSTRUCTION AT ABUTMENTS
(LOOKING FORWARD ON SURVEY)
S.R. 265 (OLD HICKORY BLVD.)

⊠ DENOTES: LIMITS OF EXCAVATION AND BACKFILL WITH GEOGRID
▨ DENOTES: LIMITS OF PABE AND SIDEWALK REMOVAL AND REPLACEMENT

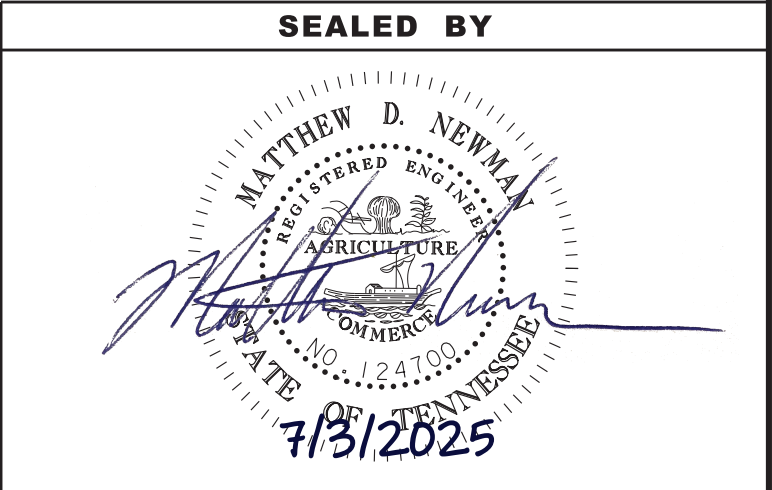


■ DENOTES: TEMPORARY SHORING MAY BE NECESSARY FOR EXCAVATION ADJACENT TO TRAFFIC LANES. THE COST OF DESIGN, INSTALLATION AND REMOVAL OF ANY REQUIRED SHORING SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 604-03.04, PAVEMENT AT BRIDGE ENDS, S.Y.

PHASE II - CONSTRUCTION AT ABUTMENTS
(LOOKING FORWARD ON SURVEY)

S.R. 265 (OLD HICKORY BLVD.)

⊠ DENOTES: LIMITS OF EXCAVATION AND BACKFILL WITH GEOGRID
▨ DENOTES: LIMITS OF PABE AND SIDEWALK REMOVAL AND REPLACEMENT















STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

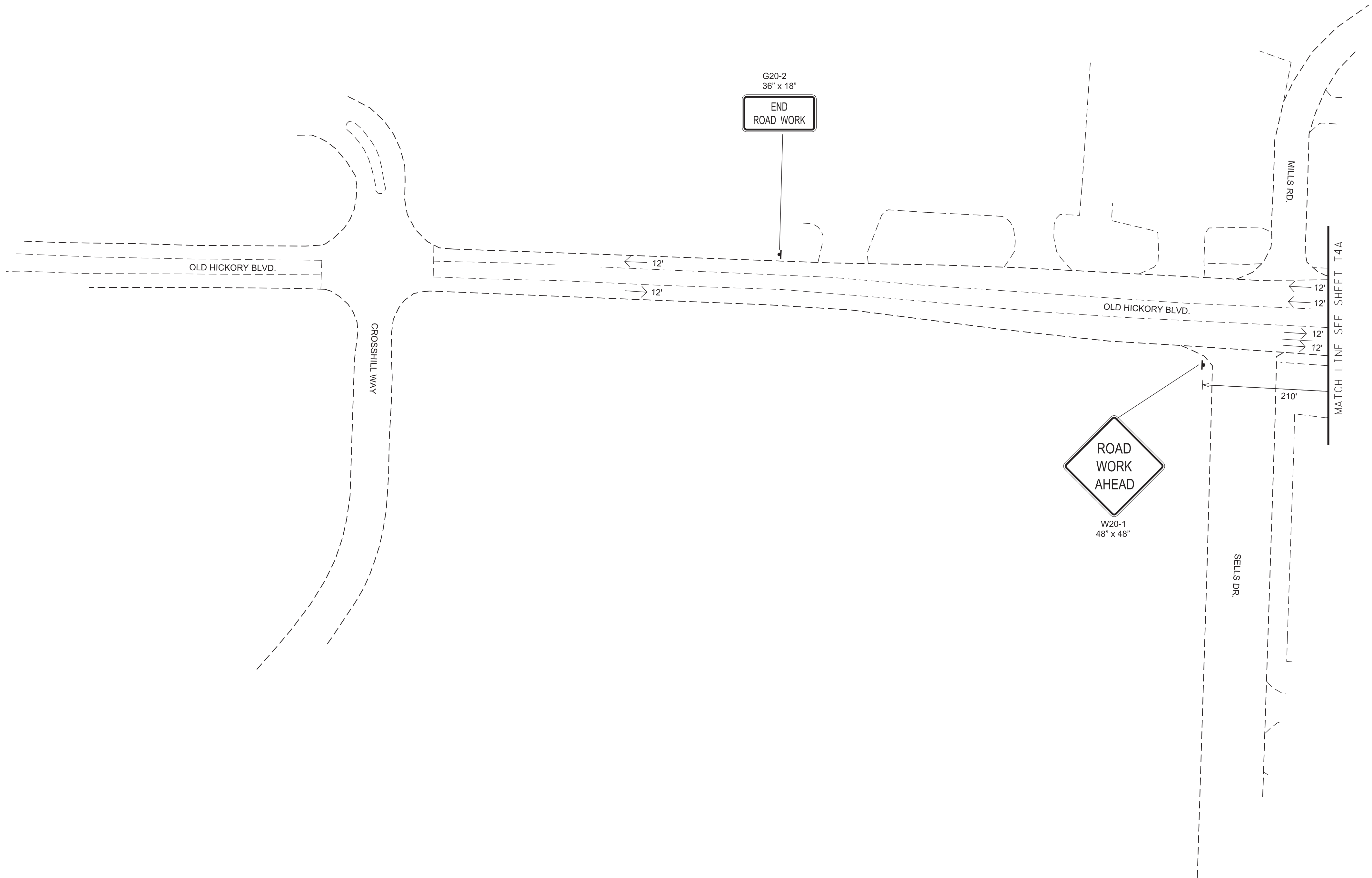
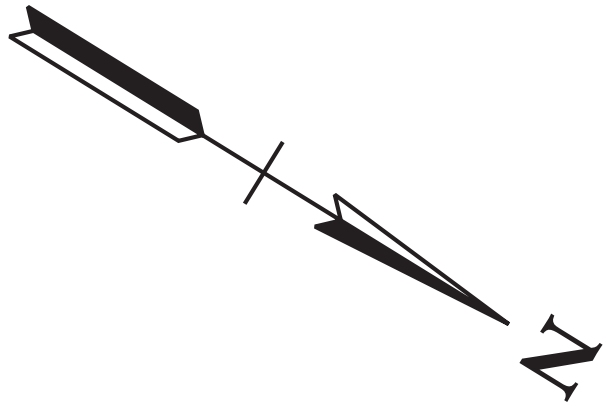
**TRAFFIC CONTROL
TYPICAL SECTIONS**

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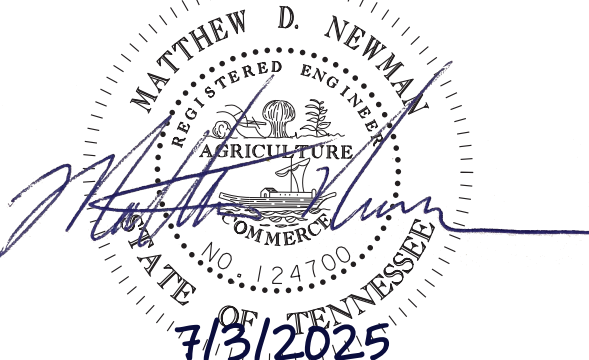
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TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE
	FLEXIBLE DRUMS (CHANNELIZING)
	SIGN (CONSTRUCTION)
	SIGN (CONSTRUCTION) (2-POST)
	TRAFFIC FLOW
	PORTABLE BARRIER RAIL
	TEMPORARY ATTENUATOR
	ARROW BOARD TYPE B
	ARROW BOARD TYPE B (DOUBLE ARROW)
	ARROW BOARD TYPE B (CAUTION)
	TEMPORARY BARRICADE (TYPE II)
	TEMPORARY BARRICADE (TYPE III)

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	19S045-M3-005	T4



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DEPARTMENT OF TRANSPORTATION

TRAFFIC
CONTROL-PHASE 1

OLD HICKORY BLVD. @ I-40
SCALE: 1"= 50'

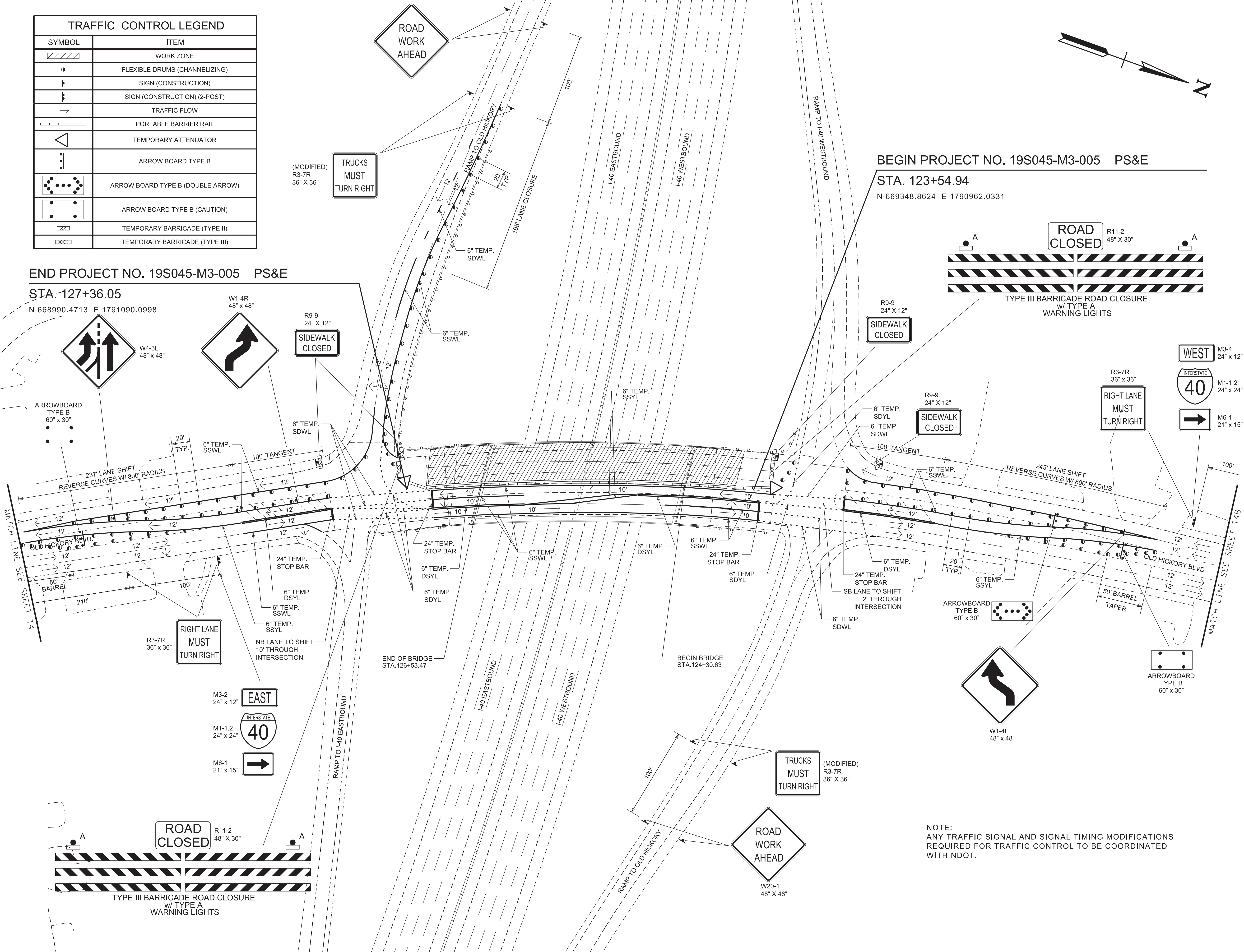
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TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE
	FLEXIBLE DRUMS (CHANNELIZING)
	SIGN (CONSTRUCTION)
	SIGN (CONSTRUCTION) (2-POST)
	TRAFFIC FLOW
	PORTABLE BARRIER RAIL
	TEMPORARY ATTENUATOR
	ARROW BOARD TYPE B
	ARROW BOARD TYPE B (DOUBLE ARROW)
	ARROW BOARD TYPE B (CAUTION)
	TEMPORARY BARRICADE (TYPE II)
	TEMPORARY BARRICADE (TYPE III)

END PROJECT NO. 19S045-M3-005 PS&E

STA. 127+36.05

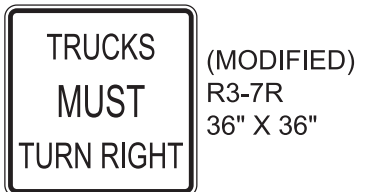
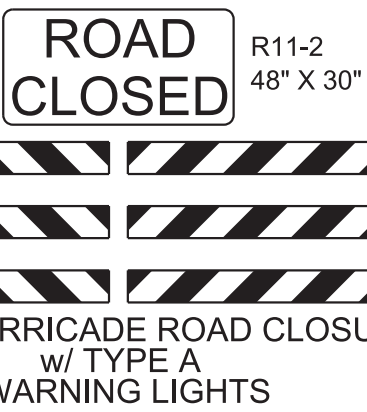
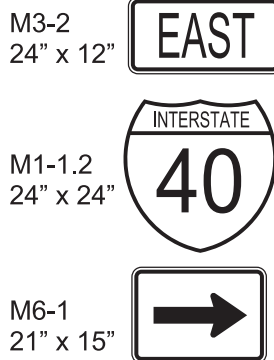
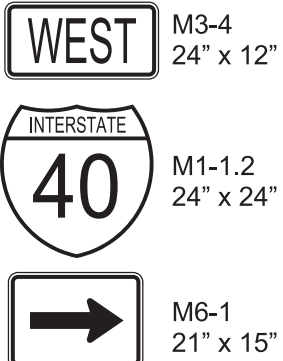
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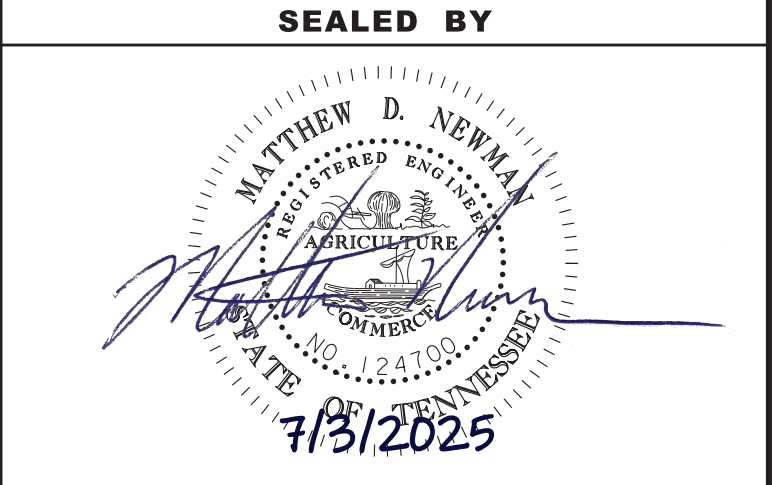
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NOTE:
ANY TRAFFIC SIGNAL AND SIGNAL TIMING MODIFICATIONS
REQUIRED FOR TRAFFIC CONTROL TO BE COORDINATED
WITH NDOT.

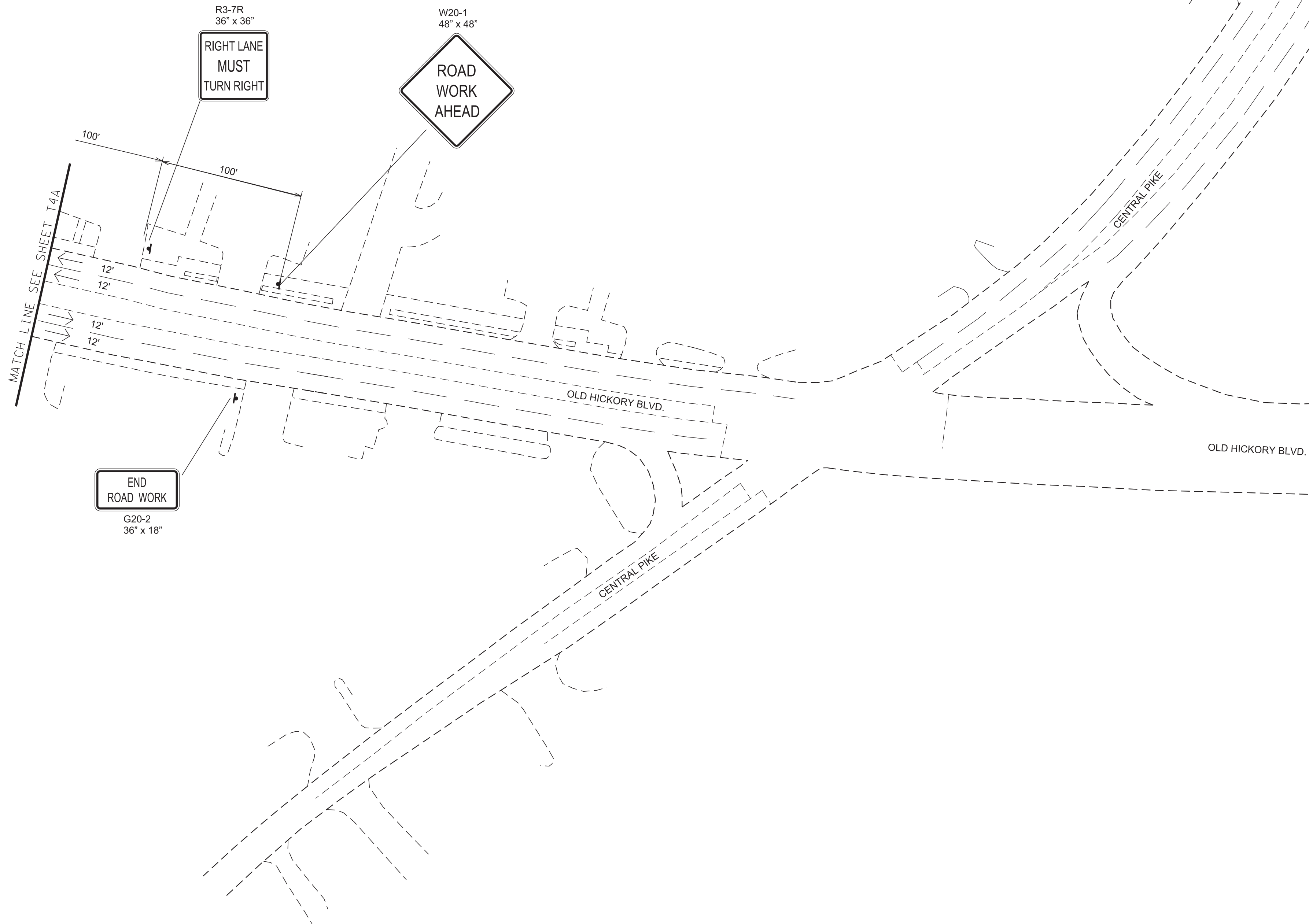
TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	19S045-M3-005	T4A



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

**TRAFFIC
CONTROL-PHASE 1**

OLD HICKORY BLVD. @ I-40
SCALE: 1"= 50'



TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE
	FLEXIBLE DRUMS (CHANNELIZING)
	SIGN (CONSTRUCTION)
	SIGN (CONSTRUCTION) (2-POST)
	TRAFFIC FLOW
	PORTABLE BARRIER RAIL
	TEMPORARY ATTENUATOR
	ARROW BOARD TYPE B
	ARROW BOARD TYPE B (DOUBLE ARROW)
	ARROW BOARD TYPE B (CAUTION)
	TEMPORARY BARRICADE (TYPE II)
	TEMPORARY BARRICADE (TYPE III)

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	19S045-M3-005	T4B

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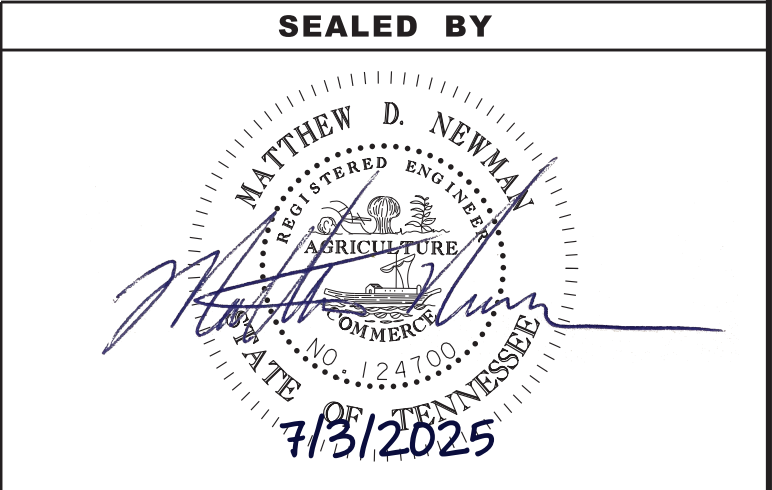
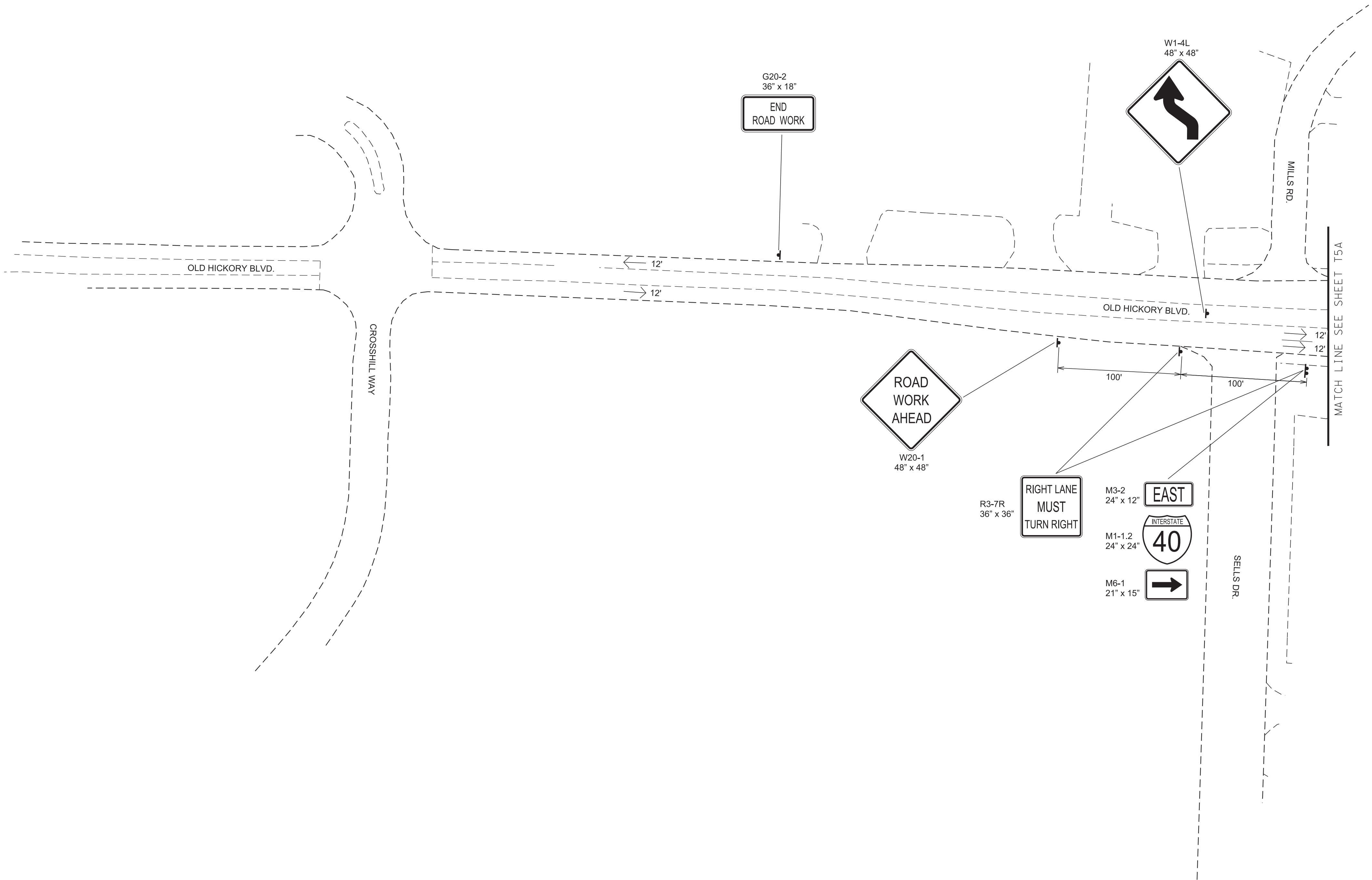
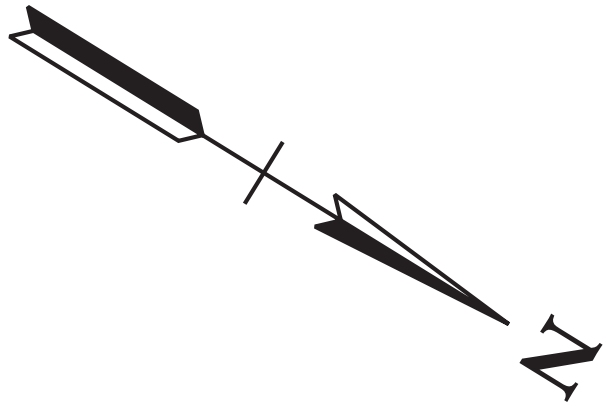
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC
CONTROL-PHASE 1

OLD HICKORY BLVD. @ I-40
SCALE: 1"= 50'

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE
	FLEXIBLE DRUMS (CHANNELIZING)
	SIGN (CONSTRUCTION)
	SIGN (CONSTRUCTION) (2-POST)
	TRAFFIC FLOW
	PORTABLE BARRIER RAIL
	TEMPORARY ATTENUATOR
	ARROW BOARD TYPE B
	ARROW BOARD TYPE B (DOUBLE ARROW)
	ARROW BOARD TYPE B (CAUTION)
	TEMPORARY BARRICADE (TYPE II)
	TEMPORARY BARRICADE (TYPE III)

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	19S045-M3-005	T5



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC
CONTROL-PHASE 2

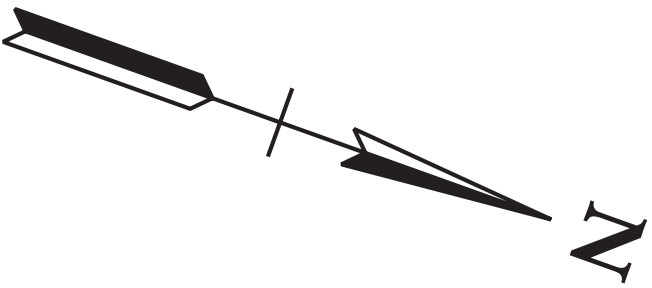
OLD HICKORY BLVD. @ I-40
SCALE: 1"= 50'

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE
	FLEXIBLE DRUMS (CHANNELIZING)
	SIGN (CONSTRUCTION)
	SIGN (CONSTRUCTION) (2-POST)
	TRAFFIC FLOW
	PORTABLE BARRIER RAIL
	TEMPORARY ATTENUATOR
	ARROW BOARD TYPE B
	ARROW BOARD TYPE B (DOUBLE ARROW)
	ARROW BOARD TYPE B (CAUTION)
	TEMPORARY BARRICADE (TYPE II)
	TEMPORARY BARRICADE (TYPE III)



(MODIFIED)
R3-7R
36" X 36"

TRUCKS
MUST
TURN RIGHT



TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	19S045-M3-005	T5A

BEGIN PROJECT NO. 19S045-M3-005 PS&E

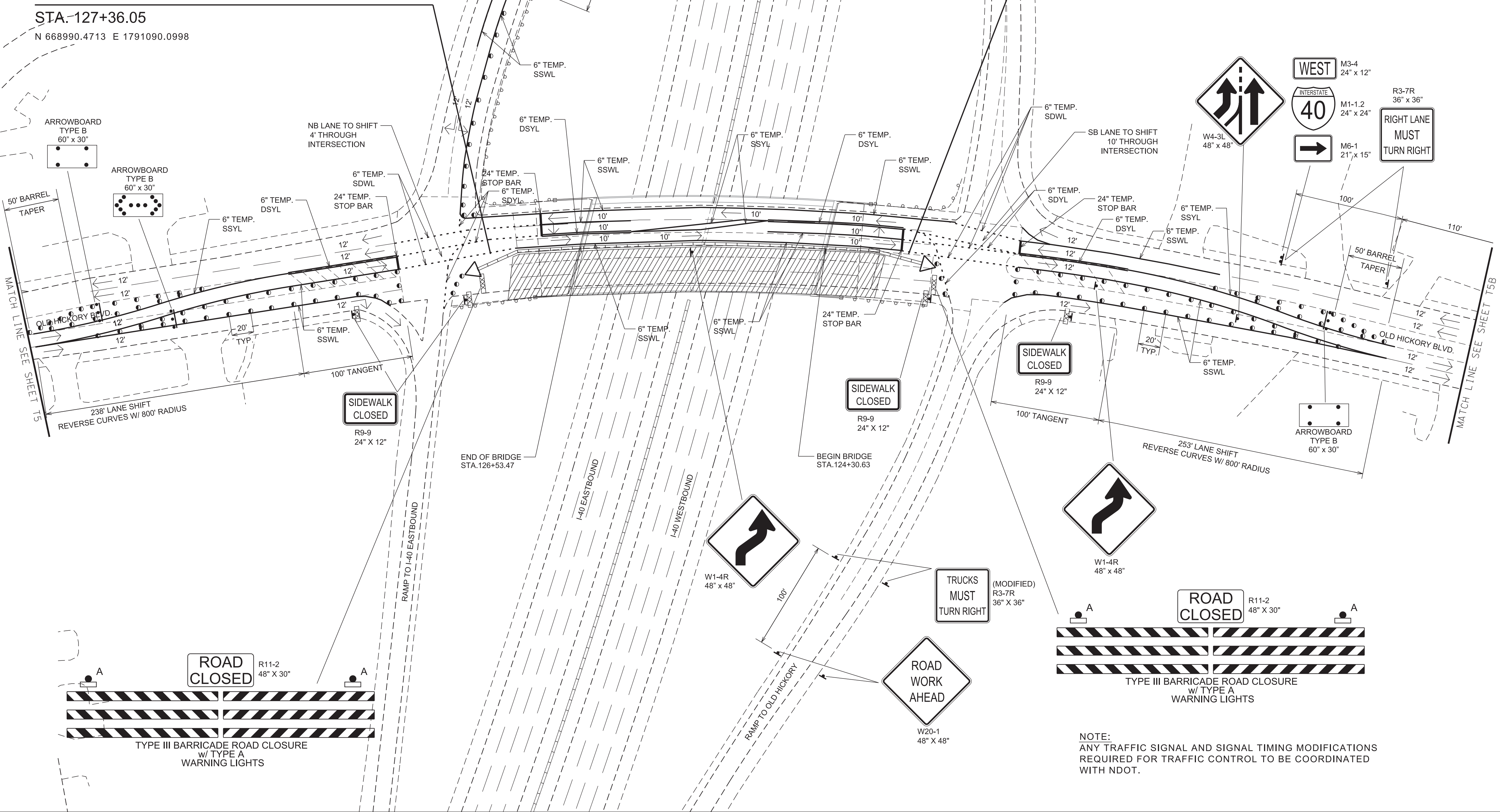
STA. 123+54.94

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END PROJECT NO. 19S045-M3-005 PS&E

STA. 127+36.05

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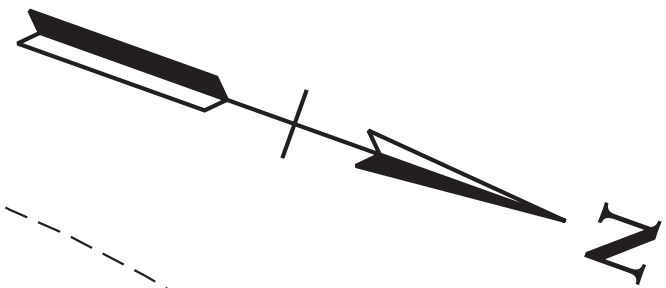
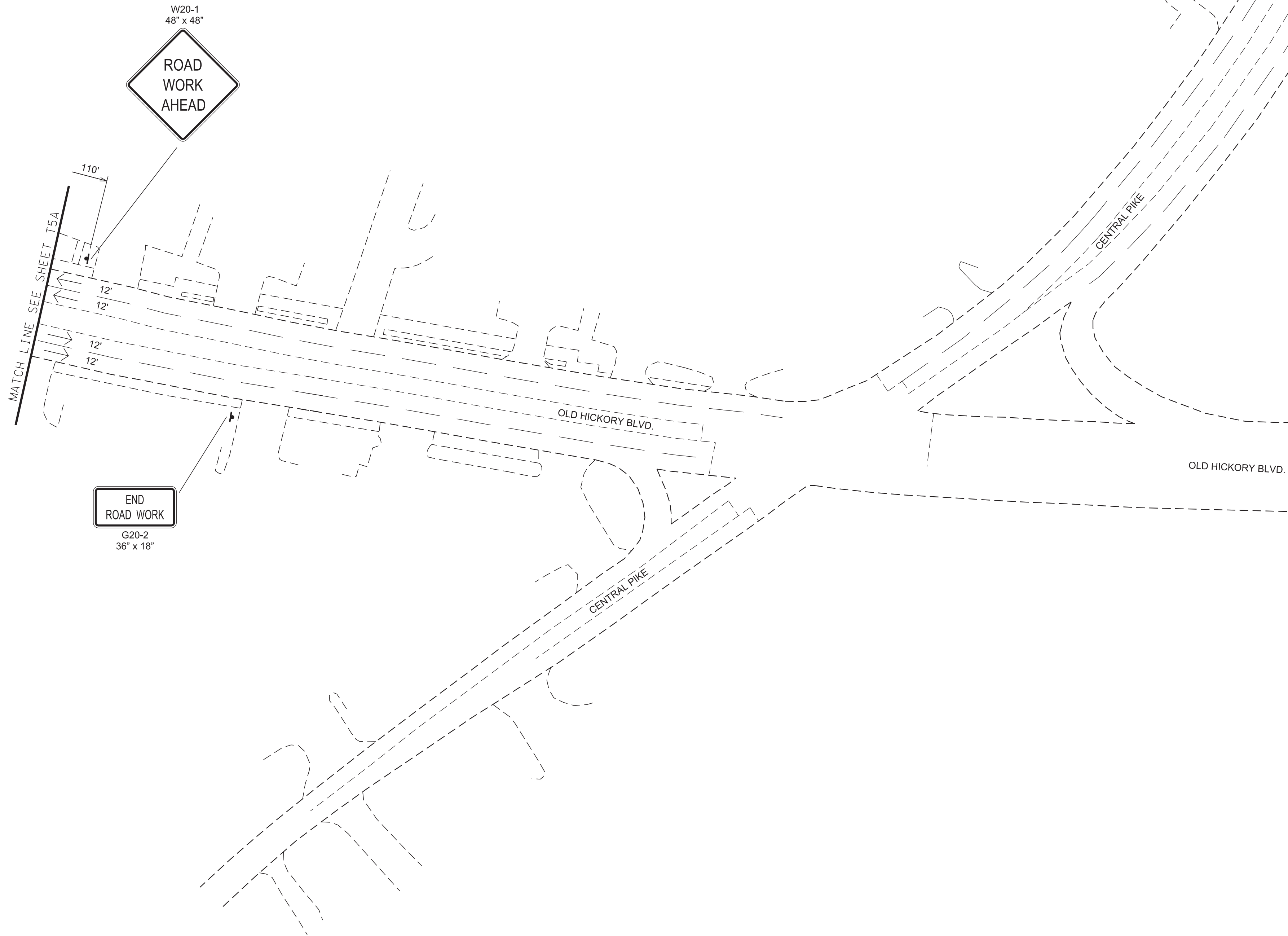
NOTE:
ANY TRAFFIC SIGNAL AND SIGNAL TIMING MODIFICATIONS
REQUIRED FOR TRAFFIC CONTROL TO BE COORDINATED
WITH NDOT.

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DEPARTMENT OF TRANSPORTATION

TRAFFIC
CONTROL-PHASE 2

OLD HICKORY BLVD. @ I-40
SCALE: 1"= 50'



TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	19S045-M3-005	T5B

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE
	FLEXIBLE DRUMS (CHANNELIZING)
	SIGN (CONSTRUCTION)
	SIGN (CONSTRUCTION) (2-POST)
	TRAFFIC FLOW
	PORTABLE BARRIER RAIL
	TEMPORARY ATTENUATOR
	ARROW BOARD TYPE B
	ARROW BOARD TYPE B (DOUBLE ARROW)
	ARROW BOARD TYPE B (CAUTION)
	TEMPORARY BARRICADE (TYPE II)
	TEMPORARY BARRICADE (TYPE III)

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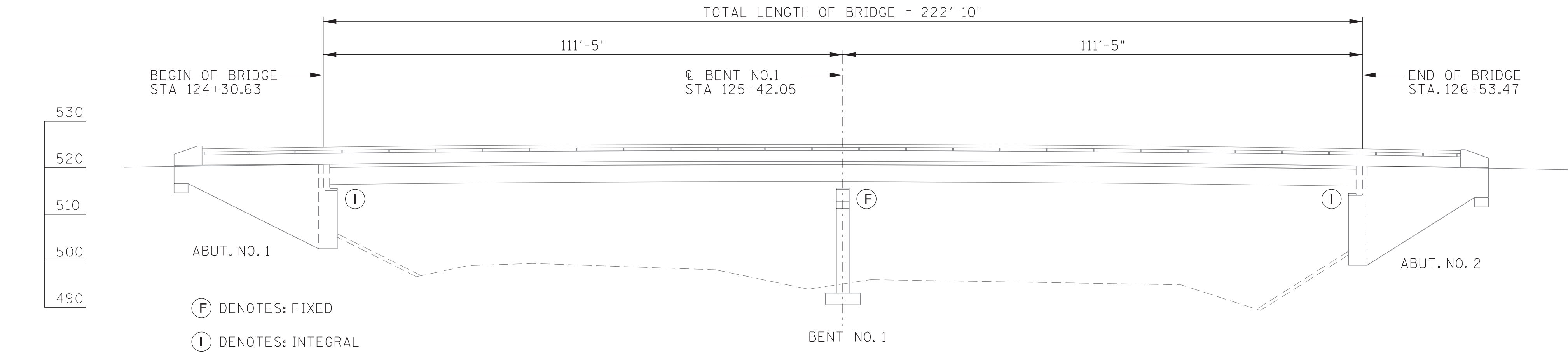
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

**TRAFFIC
CONTROL-PHASE 2**

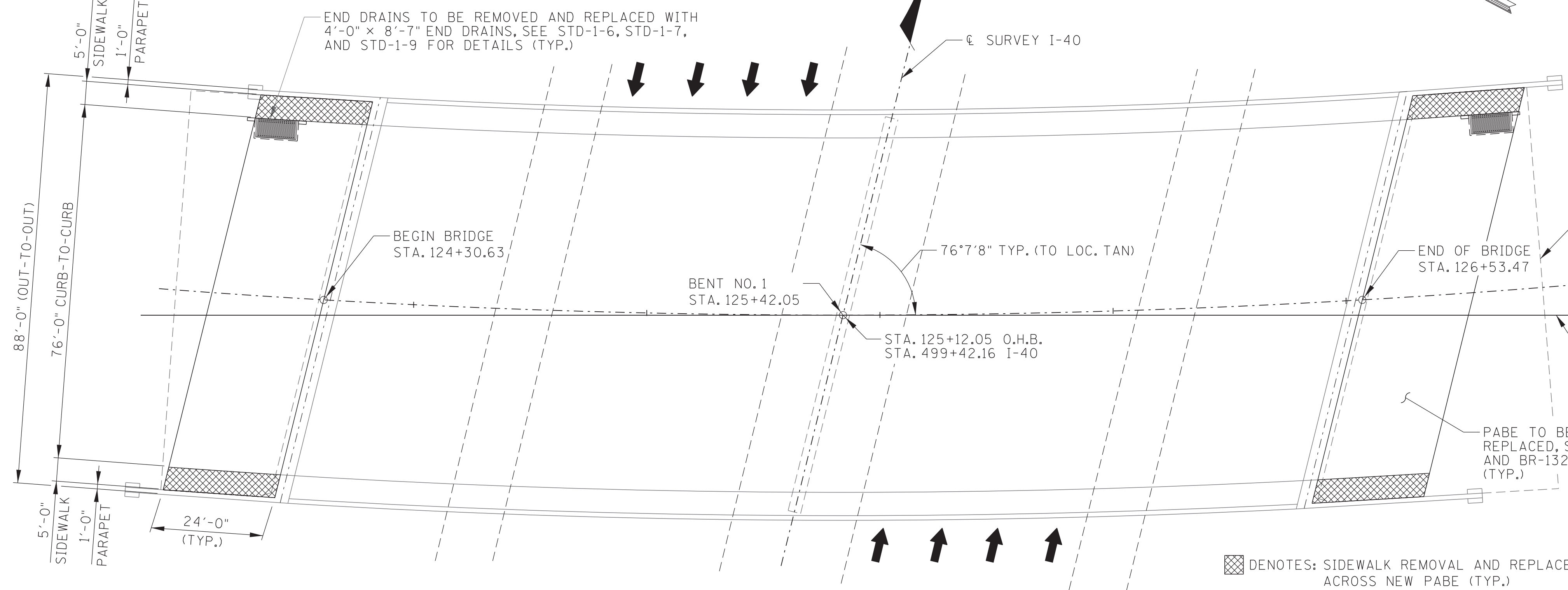
OLD HICKORY BLVD. @ I-40
SCALE: 1"= 50'

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PIN NO.:	134893.00	
DESIGN BY:	R. PARK	DATE: 07/2025
DRAWN BY:	L. MENDEZ	DATE: 07/2025
SUPERVISED BY:	D. MCCRARY	DATE: 07/2025
CHECKED BY:	D. MCCRARY	DATE: 07/2025



ELEVATION



PLAN

SCOPE OF WORK

1. INSTALL EROSION AND SEDIMENT CONTROL MEASURES AS DETAILED ON SHEET NO. 3.
2. PROVIDE REQUIRED TRAFFIC CONTROL AND PHASED CONSTRUCTION TO MAINTAIN A MINIMUM OF ONE 12'-0" LANE OF TRAFFIC IN EACH DIRECTION AT ALL TIMES ON THE EXISTING BRIDGE OF STATE ROUTE 45 AS DETAILED ON SHEET NOS. T1 THRU T6, AND BRIDGE DRAWING, NO. BR-132-845. ALL LANES OF I-40 ARE TO REMAIN FULLY OPERATIONAL FOR THE DURATION OF CONSTRUCTION OPERATIONS.
3. MILL AND REMOVE EXISTING 1 1/2" +/- OF ASPHALT OVERLAY ON PORTIONS OF THE PAVEMENT AT BRIDGE ENDS (PABE) AT ABUTMENT NO.1 AND FOR A 50 FT TRANSITION BEYOND THE PAVEMENT AT BRIDGE ENDS AT ABUTMENT NO.1 AS DETAILED IN THE PLANS ON SHEET NOS. 2B AND 3. MILLING AND REMOVAL OF THE ASPHALT OVERLAY ON THE EXISTING PABE SHALL BE PERPENDICULAR TO THE ROADWAY ALIGNMENT AND STAIR-STEPPED TRANSVERSELY ACROSS THE BRIDGE ALONG THE SKEW, UNDER NO CIRCUMSTANCES SHALL MILLING BE PERMITTED ON ANY PORTION OF THE EXISTING BRIDGE DECK.
4. MILL AND REMOVE EXISTING 1 1/4" OF EXISTING ASPHALT ON PORTIONS OF THE APPROACH ROADWAY FOR A 50 FT TRANSITION BEYOND THE PAVEMENT AT BRIDGE ENDS AT ABUTMENT NO. 2 AS DETAILED IN THE PLANS ON SHEET NOS. 2B AND 3.
5. REMOVE THE EXISTING PAVEMENT AT BRIDGE ENDS (PABE) AT BOTH ABUTMENTS NO.1 AND NO.2 WITHIN THE LIMITS SHOWN AND THE PHASED CONSTRUCTION LIMITS AS DETAILED IN THE PLANS ON DRAWING NOS. BR-132-842 AND BR-132-845. THIS INCLUDES PORTIONS OF THE EXISTING SIDEWALKS ON TOP OF THE PABE, END OF BRIDGE DRAINS, ALL CONCRETE AND REINFORCING STEEL IN THE PABE SLAB AND FOOTING, JOINT MATERIAL, BACKFILL MATERIAL TO THE DEPTH DETAILED IN TDOT STANDARD DRAWING STD-10-2.
6. INSTALL WATERPROOF MEMBRANE TO THE BACK SIDE OF ABUTMENT 1. SEE DETAILS AND NOTES ON DWG. NO. BR-132-848.
7. SPOT CLEAN WITH MECHANICAL HAND AND POWER TOOLS AND SPOT PAINT RUSTED STRUCTURAL STEEL ON EAST FASCIA GIRDER A AT ABUTMENT 1. SEE DETAILS AND NOTES ON DWG. NO. BR-132-848.
8. CONSTRUCT NEW 4' x 8'-7" BRIDGE END DRAINS AT ABUTMENT NO.1 AND NO.2 IN ACCORDANCE WITH TDOT STD. DWG. NOS. STD-1-6, STD-1-7 AND STD-1-9 WITHIN THE PHASED CONSTRUCTION LIMITS. SEE DETAILS AND NOTES ON NO. BR-132-842.
9. CONSTRUCT NEW PAVEMENT AT BRIDGE ENDS AT ABUTMENTS NO.1 AND NO.2 WITHIN THE LIMITS SHOWN AND THE PHASED CONSTRUCTION LIMITS. SEE DETAILS AND NOTES ON TDOT STD. DWG. NOS. STD-1-5, STD-10-2 AND DWG. NOS. BR-132-842 AND BR-132-845.
10. CONSTRUCT THE NEW SIDEWALK WITHIN THE LIMITS SHOWN AND THE PHASED CONSTRUCTION LIMITS. SEE DETAILS AND NOTES ON DRAWING NOS. BR-132-842 AND BR-132-845.
11. PERFORM PARTIAL AND FULL DEPTH DECK REPAIRS AND CONSTRUCT NEW THIN EPOXY OVERLAY OVER THE DECK AND NEW PABE (PER MANUFACTURER REQUIREMENTS) AS SHOWN IN THE DETAILS ON DWG. NOS. BR-132-846 AND BR-132-847.
12. REMOVE AND RESET THE EXISTING METAL GUARDRAIL AND ATTACHMENT AT THE BRIDGE ENDS AT ALL FOUR CORNERS OF THE EXISTING BRIDGES. FOR DETAILS, SEE SHEET 2B AND STANDARD S-GR-SERIES.
13. CONSTRUCT NEW ASPHALT TRANSITIONS AND RESTRIPE TO MATCH EXISTING. SEE DETAILS AND NOTES ON SHEET NO. 2B.

CONST. NO.:

PROJECT NO.		YEAR	SHEET NO.
19S045-M3-005		2025	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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LIST OF DRAWINGS

DWG. NO.	LAST REV. DATE
BR-132-842	
BR-132-843	
BR-132-844	
BR-132-845	
BR-132-846	
BR-132-847	
BR-132-848	
BR-132-849	

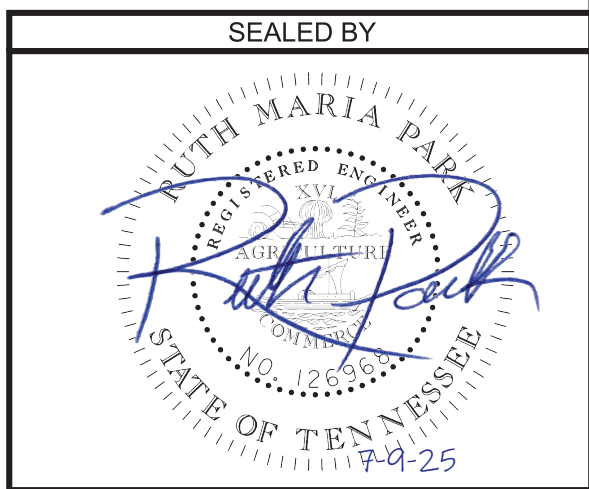
LIST OF STANDARD DRAWINGS

DWG. NO.	LAST REV. DATE
STD-1-5	06-05-2023
STD-1-6	05-10-2021
STD-1-7	05-10-2021
STD-1-9	05-10-2021
STD-10-2	06-05-2023

LIST OF REFERENCE DRAWINGS

DRAWING NO.	ORIGINAL BRIDGE DRAWINGS
M-246-71 THRU M-246-86A	EXISTING BRIDGE PLANS

NOTE: ALL DIMENSIONS AND ELEVATIONS ARE BASED ON EXISTING PLAN INFORMATION, FIELD MEASUREMENTS, OR CONTRACTOR MEASUREMENTS. PRIOR TO CONSTRUCTION, IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO FIELD VERIFY ALL DIMENSIONS.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
LAYOUT OF BRIDGE
TO BE REPAIRED
OLD HICKORY BOULEVARD
OVER INTERSTATE 40 EAST
BR. NO. 19-SR045-17.92
FED. ID. NO. 19I00400141
DAVIDSON COUNTY
2025

BR-132-842

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ESTIMATED BRIDGE QUANTITIES				
	ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY
(1)	202-04.01	REMOVAL OF STRUCTURES (PAVEMENT AT BRIDGE ENDS)	LS	1
(2)	603-02.20*	SPOT PAINTING EXISTING STEEL STRUCTURES	S.F	35
(3)	604-02.03	EPOXY COATED REINFORCING STEEL	LB.	1114
(4)(5)	604-03.01	CLASS A CONCRETE (BRIDGES)	C.Y	10
(6)(7)	604-03.04	PAVEMENT @ BRIDGE ENDS	S.Y.	458
(8)	604-04.04	BRIDGE END DRAINS (4'X8')	EACH	2
(9)	604-10.14	REMOVE EXISTING WEARING SURFACE	LS	1
(10)	604-10.30*	BRIDGE DECK REPAIRS (FULL DEPTH OF SLAB)	S.Y.	35
(11)	604-10.50*	BRIDGE DECK REPAIRS (PARTIAL DEPTH OF SLAB)	S.Y.	35
(12)	617-04.01	TYPE I THIN OVERLAY (EPOXY URETHANE)	S.Y.	2287

* DENOTES: ITEM CAN BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.

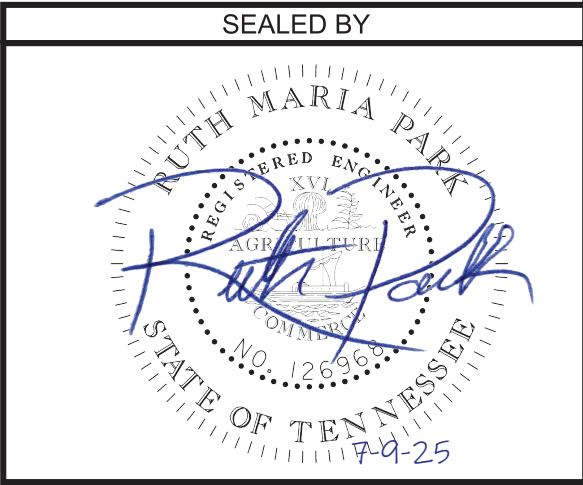
FOOTNOTES:

- (1) INCLUDES ALL COSTS ASSOCIATED WITH THE REMOVAL OF THE PAVEMENT AT BRIDGE ENDS AT ABUTMENTS NOS. 1 AND 2 INCLUDING THE SIDEWALK, AND EXISTING END DRAINS WITHIN THE LIMITS OF CONSTRUCTION.
- (2) INCLUDES ALL LABOR, MATERIALS, AND INCIDENTALS REQUIRED TO CLEAN AND PAINT AREAS OF EXISTING STRUCTURAL STEEL AS INDICATED IN THE PLANS OR DESIGNATED BY THE ENGINEER. PAYMENT SHALL BE FULL COMPENSATION FOR OBTAINING ALL PERMITS AND APPROVALS. ALL SURFACE PREPARATION, CONTAINMENT, FURNISHING, AND APPLYING PAINT IS INCLUDED AND SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS. AREAS TO BE SPOT CLEANED AND PAINTED TO BE MARKED AND QUANTIFIED FOR APPROVAL BY THE ENGINEER PRIOR TO BEGINNING THE WORK. ITEM NO. 603-02.20 MAY BE INCREASED OR DECREASED AT THE DISCRETION OF THE ENGINEER. SEE. DWG. NO. BR-132-848.
- (3) INCLUDES ALL COSTS FOR PLACEMENT OF EPOXY COATED REINFORCING STEEL IN SIDEWALKS AS SHOWN ON DWG. NOS. BR-132-846 AND BR-132-849.
- (4) INCLUDES ALL COSTS FOR PLACEMENT OF CONCRETE SIDEWALK ON PAVEMENT AT BRIDGE ENDS. FOR DETAILS AND LOCATIONS, SEE DWG. NOS. BR-132-842 AND BR-132-846.
- (5) THE COST OF BITUMINOUS FIBERBOARD AND ALL MISCELLANEOUS JOINT MATERIAL TO BE INCLUDED IN THE UNIT PRICE BID FOR OTHER ITEMS. SEE DWG. NO. BR-132-836.
- (6) PRIOR TO CONSTRUCTION OF THE PAVEMENT AT BRIDGE ENDS, THE CONTRACTOR SHALL SUBMIT A PROPOSED BILL OF STEEL TO THE ENGINEER FOR APPROVAL. THIS SHALL INCLUDE THE COST OF LABOR AND MATERIALS ASSOCIATED WITH BACKFILL EXCAVATION AND PLACEMENT OF THE NEW PAVEMENT AT BRIDGE END. SEE STD-1-5 AND STD-10-2.
- (7) THE COST OF ALL LABOR AND MATERIAL FOR PLACING THE WATER PROOF MEMBRANE TO BE INCLUDED IN THE UNIT PRICE BID FOR OTHER ITEMS. SEE DWG. NO. BR.-132-848 AND THE TDOT QUALIFIED PRODUCTS LIST 2
- (8) INCLUDES COST OF ALL LABOR AND MATERIALS REQUIRED TO INSTALL BRIDGE END DRAINS. SEE DWG. NO. BR-132-842, AND STANDARD DRAWINGS STD-1-6, STD-1-7, AND STD-1-9 FOR DETAILS.
- (9) INCLUDES COST OF ALL LABOR AND MATERIALS REQUIRED TO REMOVE AND DISPOSE OF THE EXISTING ASPHALT WEARING SURFACE ON THE PABE AT ABUTMENT NO. 1
- (10) INCLUDES COST OF ALL LABOR AND MATERIALS TO PERFORM FULL DEPTH DECK REPAIRS. CONTRACTOR SHALL TAKE EXTREME CAUTION TO AVOID DAMAGING STAY IN PLACE DECK FORMS. SEE DETAIL ON DWG. NO. BR-132-847.
- (11) INCLUDES COST OF ALL LABOR AND MATERIALS TO PERFORM PARTIAL DEPTH DECK REPAIRS. SEE DETAIL ON DWG. NO. BR-132-847.
- (12) INCLUDES COST OF ALL LABOR AND MATERIALS NECESSARY TO PLACE A TYPE I THIN OVERLAY (EPOXY-URETHANE) TO THE BRIDGE DECK. SEE NOTES ON DWG. NO. BR-132-847 AND THE TDOT QUALIFIED PRODUCTS LIST 23 SECTION 02.

PIN NO.:	134893.00	
DESIGN BY:	R. PARK	DATE: 07/2025
DRAWN BY:	L. MENDEZ	DATE: 07/2025
SUPERVISED BY:	D. MCCRARY	DATE: 07/2025
CHECKED BY:	D. MCCRARY	DATE: 07/2025



CONST. NO.:			
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19S045-M3-005		2025	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
ESTIMATED BRIDGE
QUANTITIES
OLD HICKORY BOULEVARD
OVER INTERSTATE 40 EAST
BR. NO. 19-SR045-17.92
FED. ID. NO. 19I00400141
DAVIDSON COUNTY
2025

GENERAL NOTES

SPECIFICATIONS & LOADING

- (1) **CONSTRUCTION SPECIFICATIONS:** STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION (JANUARY 1, 2021 EDITION), AND THE 4TH EDITION (2017) AASHTO LRFD BRIDGE CONSTRUCTION SPECIFICATIONS WITH INTERIMS.
- (2) **DESIGN SPECIFICATIONS:** 9TH EDITION (2020) AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS WITH INTERIMS, AND THE 2ND EDITION (2011) AASHTO GUIDE SPECIFICATIONS FOR LRFD SEISMIC BRIDGE DESIGN WITH INTERIMS.

STEEL, CONCRETE, REINFORCING, AND FORMING

- (3) **REINFORCING STEEL:** SHALL BE ASTM A615 GRADE 60 UNLESS NOTED OTHERWISE. SEE SECTION 604 AND 907 OF THE STANDARD SPECIFICATIONS.
- (4) **CONCRETE:** TO BE CLASS A (CAST-IN-PLACE) F'C = 3000 PSI EXCEPT AS NOTED OTHERWISE.
- (5) **HIGH EARLY STRENGTH CONCRETE:** THE MIX IS TO MEET THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS, CLASS X. THE CEMENT CONTENT SHALL BE A MINIMUM OF 714 LBS. THE WATER-CEMENT RATIO SHALL BE A MAXIMUM OF 0.40. DESIGN AIR CONTENT SHALL BE 6% WITH $\pm 2\%$ ACCEPTANCE RANGE IN THE FIELD. SLUMP SHALL BE 3 ± 1 INCHES. IF USING A TYPE A, F, OR G WATER REDUCER, THE SLUMP SHALL BE MAXIMUM OF 8 INCHES. NO FLY ASH REPLACEMENT WILL BE PERMITTED. THE MINIMUM 28 DAY COMPRESSIVE STRENGTH SHALL BE 3,500 PSI. TRAFFIC SHALL NOT BE PERMITTED ON ANY OF THE REPAIRED AREAS UNTIL TEST SPECIMENS ATTAIN A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI AND THE CONCRETE HAS BEEN IN PLACE A MINIMUM OF TEN (10) DAYS.
- (6) **CONCRETE CURING:** ALL CONCRETE IN REPAIR AREAS SHALL BE CURED ACCORDING TO THE STANDARD SPECIFICATIONS.

MISCELANEOUS GENERAL NOTES

- (7) **SHOP DRAWINGS:** REFER TO SECTION 105.02 OF THE STANDARD SPECIFICATIONS. IF USING PAPER COPIES, SHOP DRAWINGS ARE TO BE SENT TO THE BRIDGE REPAIR OFFICE IN THE DIVISION OF STRUCTURES, FOR ELECTRONIC SUBMITTALS, SEE SECTION 105.02 OF THE STANDARD SPECIFICATIONS. EACH SHOP DRAWING SHALL CONTAIN IN THE TITLE BLOCK THE FOLLOWING: THE STATE PROJECT NUMBER, COUNTY, BRIDGE NAME, BRIDGE NUMBER (OR STRUCTURE TYPE AND NUMBER), STATION, AND CONTRACT NUMBER. SHOP DRAWINGS WITH TITLE BLOCKS NOT INCLUDING THE FOREGOING IDENTIFICATION WILL BE RETURNED FOR CORRECTION BEFORE ANY REVIEWS FOR APPROVAL ARE CONDUCTED.
- (8) **DEMOLITION:** THE CONTRACTOR SHALL TAKE SPECIAL CARE TO PROTECT ANY PARTS OF THE STRUCTURE THAT ARE NOT TO BE REMOVED SPECIFICALLY. FOR FULL DEPTH SLAB REMOVAL, EXCEPT OVER BEAMS, THE MAXIMUM HAMMER SIZE IS 90 POUND CLASS. FOR PARTIAL DEPTH SLAB REMOVAL AND ANY WORK OVER THE BEAMS, THE MAXIMUM HAMMER SIZE IS 60 POUND CLASS; CHIPPING HAMMERS OF THE 15 POUND CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL. SAWING OR CUTTING OF THE CONCRETE IS ACCEPTABLE AS LONG AS ANY SPECIFIED PROJECTION OF THE EXISTING REINFORCING STEEL IS MAINTAINED. EXPANSION JOINT REMOVAL SHALL FOLLOW THE SAME RESTRICTIONS AS FULL DEPTH SLAB REMOVAL. ALL DEVICES PROPOSED FOR CONCRETE DEMOLITION SHALL MEET THE APPROVAL OF THE ENGINEER.
- (9) THE CONTRACTOR IS NOT ALLOWED TO USE A HYDRAULIC RAM MOUNTED ON A BACKHOE (COMMONLY CALLED A HOE RAM), MINI EXCAVATOR, OR OTHER EQUIPMENT FOR ANY CONCRETE REMOVAL.

- (10) **BLAST CLEANING:** SHALL BE IN ACCORDANCE WITH STANDARD SPECIFICATION 603.05(B), EXCEPT AS MODIFIED BY THIS NOTE. ALL STRUCTURAL STEEL SHALL BE BLAST CLEANED. THE BLAST CLEANING SHALL REMOVE ALL EXISTING FINISH COAT. THE TIGHTLY ADHERING EXISTING INORGANIC ZINC PRIMER COAT MAY REMAIN.
- (11) **APPLICATION:** THE COATING APPLICATOR SHALL FOLLOW THE MANUFACTURER'S PRINTED INSTRUCTIONS AND SHALL HAVE THESE INSTRUCTIONS ON SITE DURING THE COURSE OF THE WORK.
- (12) **CONTAINMENT AND DISPOSAL:** ACCORDING TO OUR RECORDS, THE EXISTING PAINT SYSTEM CONTAINS AN INORGANIC ZINC PRIMER AND URETHENE TOP COAT. THE CONTRACTOR SHALL CONTAIN AND DISPOSE OF ALL PAINT REMOVAL WASTE IN ACCORDANCE WITH SECTION 603.13 OF THE STANDARD SPECIFICATIONS.

SPOT PAINTING AREAS NOTES

- (13) **SPOT CLEANING AND PAINTING:** AREAS REQUIRING REPAINTING OF STEEL SHALL BE CLEANED IN ACCORDANCE WITH SECTION 603.05(B.1) OF THE STANDARD SPECIFICATIONS. HAND (SSPC-SP 2) OR POWER (SSPC-SP 3) TOOL CLEANING SHALL REMOVE ALL RUST, SCALE, LOOSE PAINT, AND DIRT. THESE AREAS SHALL BE SPOT PRIMED IMMEDIATELY AFTER CLEANING. CLEANED AND PRIMED STEEL SHALL RECEIVE A SYSTEM "B" (QPL 3.005) PAINT SYSTEM IN ACCORDANCE WITH SECTION 603.06(B) OF STANDARD SPECIFICATIONS. THE COLOR OF THE FINISH COAT SHALL COMPLY WITH AMS-STD-595A, FEDERAL STANDARD NO. 30059, BROWN. SEE SECTIONS 603 AND 910 OF THE STANDARD SPECIFICATIONS.
- (14) SEE THE TDOT QUALIFIED PRODUCTS LIST 3.005 OR 3.006 FOR ACCEPTABLE COATINGS FOR THE PAINT SYSTEM. THE SAME MANUFACTURER SHALL SUPPLY ALL PRODUCTS USED, INCLUDING THINNERS.

SPECIAL NOTES

- (15) NOTE: CONTRACTOR IS RESPONSIBLE FOR AND SHALL TAKE ALL PRECAUTIONS TO ENSURE THE STABILITY OF THE STRUCTURE DURING THE REPAIR.
- (16) NOTE: THE CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS AND QUANTITIES PRIOR TO ORDERING ANY MATERIALS.

CONST. NO.:

PROJECT NO.		YEAR	SHEET NO.
19S045-M3-005		2025	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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PIN NO: 134893.00		
DESIGN BY: R. PARK		DATE: 07/2025
DRAWN BY: L. MENDEZ		DATE: 07/2025
SUPERVISED BY: D. MCCRARY		DATE: 07/2025
CHECKED BY: D. MCCRARY		DATE: 07/2025

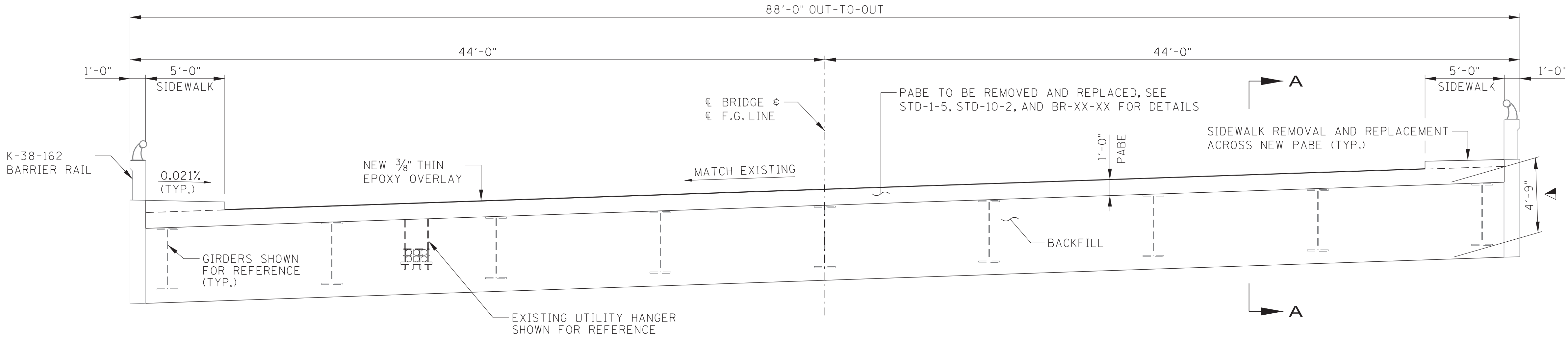


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BRIDGE
GENERAL NOTES
OLD HICKORY BOULEVARD
OVER INTERSTATE 40 EAST
BR. NO. 19-SR045-17.92
FED. ID. NO. 19100400141
DAVIDSON COUNTY
2025

BR-132-844

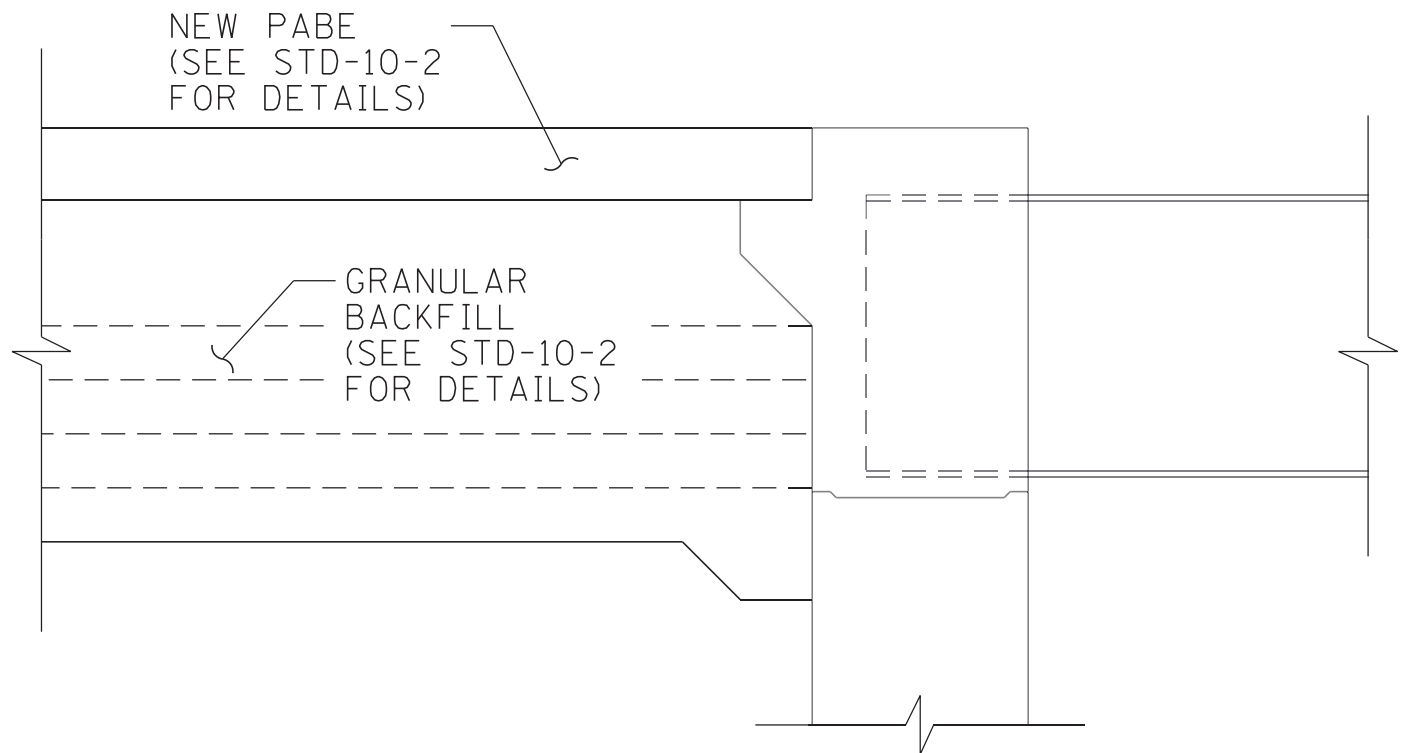
CONST. NO.:

PROJECT NO.		YEAR	SHEET NO.
19S045-M3-005		2025	
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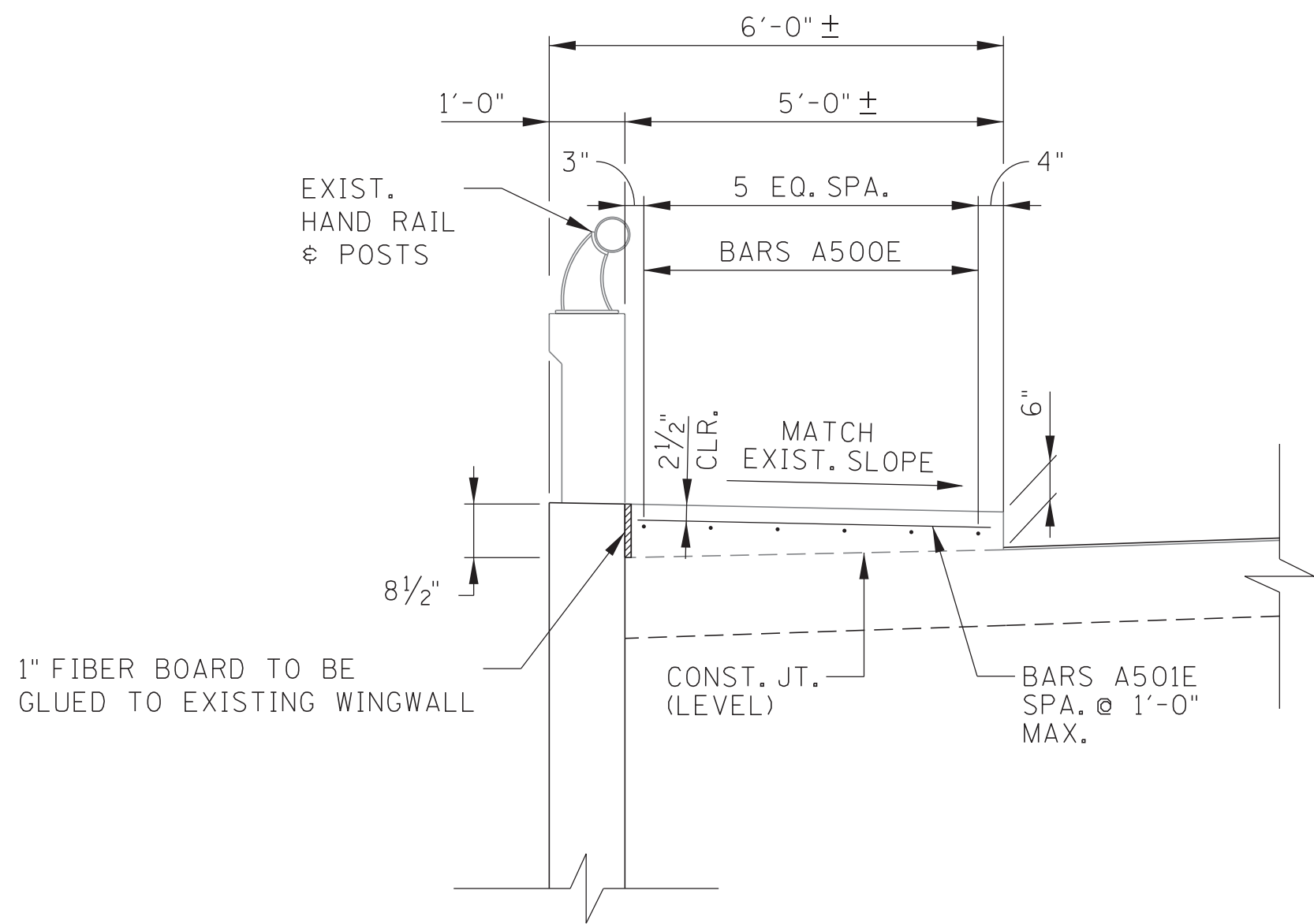


▲ DENOTES: LIMITS OF EXCAVATION
AND BACKFILL

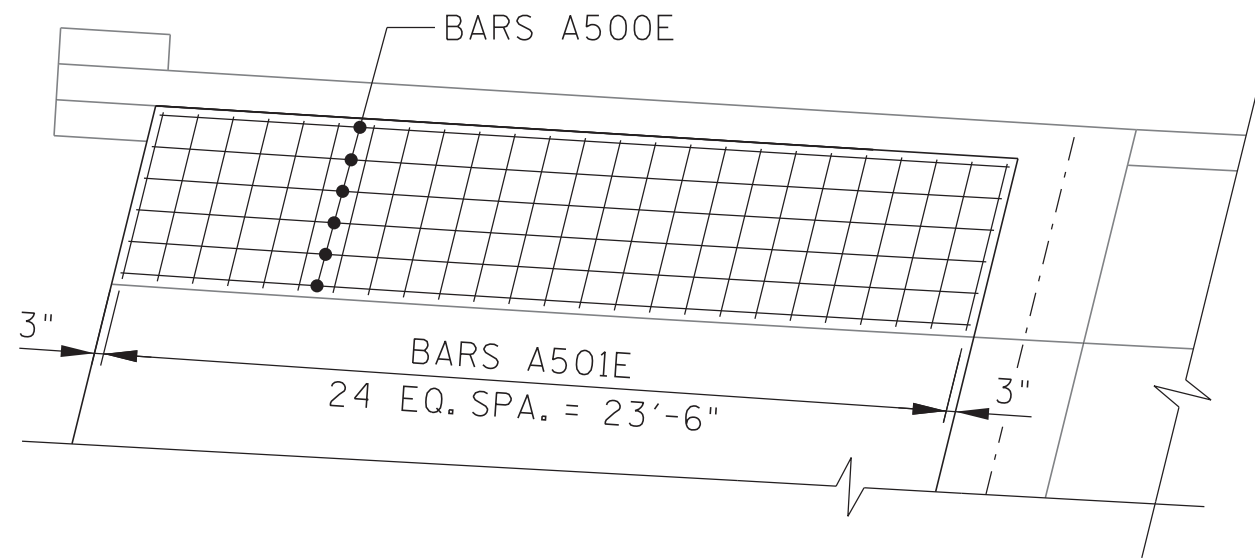
TYPICAL SECTION PAVEMENT AT BRIDGE ENDS
(LOOKING FORWARD ON SURVEY)



SECTION A-A
(ABUTMENT NOS. 1 AND 2)

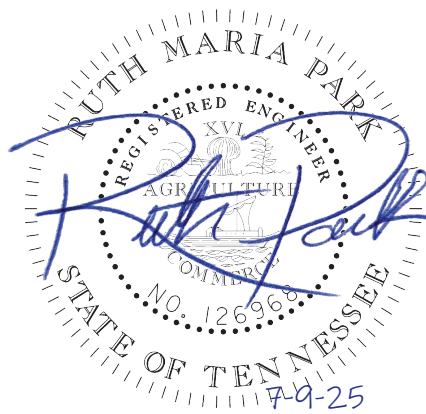


SIDEWALK DETAIL



PLAN OF SIDEWALK REINFORCEMENT
(TYP. 4 LOCATIONS)

SEALED BY



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
SUPERSTRUCTURE REPAIR
DETAILS
OLD HICKORY BOULEVARD
OVER INTERSTATE 40 EAST
BR. NO. 19-SR045-17.92
FED. ID. NO. 19I00400141
DAVIDSON COUNTY
2025

BR-132-846

PIN NO.: 134893.00
DESIGN BY: R. PARK DATE: 07/2025
DRAWN BY: L. MENDEZ DATE: 07/2025
SUPERVISED BY: D. MCCRARY DATE: 07/2025
CHECKED BY: D. MCCRARY DATE: 07/2025



THIN EPOXY OVERLAY NOTES:

THE TOP OF THE NEW OVERLAY SHALL CONFORM TO THE CROSS SLOPE AND GRADE OF THE BRIDGE.

TYPE I THIN EPOXY OVERLAY SYSTEM - USE DECK PRETREATMENT/PRIMER PER MANUFACTURER'S RECOMMENDATION AND 2 LIFTS OF AN EPOXY URETHANE COPOLYMER AND AGGREGATE. TYPE I OVERLAY SHALL BE APPLIED MECHANICALLY USING METERED EQUIPMENT; HAND MIXING IS NOT PERMITTED.

THIN OVERLAY SYSTEM SHALL BE FROM THE QUALIFIED PRODUCTS LIST 23, SECTION 02. MINIMUM OVERLAY THICKNESS SHALL BE 3/8".

APPLICATION EQUIPMENT SHOULD:

- A) BE CAPABLE OF METERING, MIXING AND DISTRIBUTING THE POLYMER AND PRETREATMENT TO MANUFACTURER'S RECOMMENDATION.
- B) USE AN APPLICATION MACHINE THAT FEATURES POSITIVE DISPLACEMENT VOLUMETRIC METERING PUMPS CONTROLLED BY A HYDRAULIC POWER UNIT.
- C) STORE COMPONENTS IN TEMPERATURE CONTROLLED RESERVOIRS CAPABLE OF MAINTAINING 100 DEGREES FAHRENHEIT (PLUS OR MINUS 10 DEGREES) TO INSURE OPTIMAL MIXING.
- D) CHECK MIXING RATIO AT THE PUMP OUTLETS AS WELL AS CYCLE COUNTING CAPABILITIES TO MONITOR OUTPUT ON STANDARD FEATURES.
- E) USE MOTIONLESS IN-LINE MIXING SO AS TO NOT OVERLY SHEAR THE MATERIAL TO ENTRAP AIR IN THE MIX.
- F) MAXIMIZE MATERIAL WORKING TIME BY MIXING IT IMMEDIATELY BEFORE DISPENSING.

AGGREGATE SHALL BE ANGULAR, HAVING LESS THAN 0.2% MOISTURE AND FREE OF DIRT, CLAY, ASPHALT AND OTHER FOREIGN OR ORGANIC MATERIALS. AGGREGATE FOR ALL LAYERS SHALL BE BAUXITE OR FLINT ROCK PRODUCTS FLINT AND MEETS THE FOLLOWING GRADATION:

SIEVE SIZE	% PASSING
NO. 6	95-100
NO. 10	10-35
NO. 20	95-100

FOR THE CLOSURE POUR MATERIAL, SEE MANUFACTURER'S RECOMMENDATION ON CURE TIME AND STRENGTH BEFORE THE OVERLAY IS PLACED.

SEE MANUFACTURER'S RECOMMENDATIONS FOR REQUIRED AMBIENT AND SURFACE TEMPERATURES AND HUMIDITY LIMITS FOR APPLICATION.

THE CONCRETE DECK SURFACE SHALL BE CLEANED BY SHOTBLASTING TO REMOVE ANY OIL, DIRT, RUBBER, TRAFFIC STRIPPING, AND ANY OTHER POTENTIAL DETRIMENTAL MATERIAL SUCH AS CURING COMPOUND AND LAITANCES, WHICH THE MANUFACTURER AND ENGINEER'S OPINION WOULD PREVENT PROPER BONDING AND CURING OF THE MATERIAL. IN AREAS WHERE SHOTBLASTING EQUIPMENT CAN NOT REACH (I.E., ALONG CURBS AND BRIDGE RAILS) SANDBLASTING IS PERMITTED TO AN EXTENT TO THE ENGINEER'S AND MANUFACTURER'S APPROVAL, IMMEDIATELY BEFORE APPLICATION. ALL PREPARED SURFACES SHALL BE CLEANED WITH COMPRESSED AIR OR VACUUMED TO REMOVE DUST AND DEBRIS.

THE CONTRACTOR IS TO PREVENT THE TRACKING OF TACKCOAT AND CONSTRUCTION DEBRIS ACROSS THE BRIDGE DECK PRIOR TO APPLICATION OF THIN EPOXY OVERLAY. MILLING THE BRIDGE DECK WILL NOT BE AN OPTION FOR TACKCOAT AND DEBRIS REMOVAL.

ALL SURFACES THAT ARE TREATED SHALL BE DRY AT THE TIME OF APPLICATION. THE OVERLAY SHALL NOT BE APPLIED WHEN IT HAS RAINED 24 HOURS PRIOR TO, OR RAIN IS FORECASTED WITHIN 8 HOURS AFTER, APPLICATION. THE MOISTURE CONTENT IN THE DECK SUBSTRATE SHALL BE TESTED. MOISTURE IS NOT TO EXCEED 4.5 PERCENT WHEN MEASURED BY ELECTRONIC METER OR BY TAPING A 4 FT X 4 FT POLYETHYLENE SHEET TO THE CONCRETE DECK, IF MOISTURE COLLECTS UNDER THE PLASTIC IN LESS TIME THAN IT WOULD TAKE FOR THE EPOXY TO CURE. IF EITHER TEST SHOWS EXCESS MOISTURE, THE DECK SHALL CONTINUE TO DRY BEFORE APPLICATION PROCEEDS.

BLUSHING (A WAXY SURFACE COATING ON THE EPOXY) IS CAUSED BY THE REACTION OF MOISTURE WITH THE HARDENING AGENT. BLUSHING CREATES A SURFACE THAT MAKES FUTURE LAYERS DIFFICULT TO ADHERE. LIFTS THAT SHOW SIGNS OF BLUSHING SHALL BE REMOVED AND REPLACED PRIOR TO APPLICATION OF THE NEXT LAYER. THE COST TO REMOVE AND REPLACE AREAS SHALL BE AT THE CONTRACTOR'S EXPENSE.

TRAFFIC OTHER THAN APPLICATION EQUIPMENT, SHALL NOT BE ALLOWED ON ANY PORTION OF THE DECK THAT HAS BEEN SHOTBLASTED OR WHERE PART OF THE APPLICATION HAS BEEN PLACED.

THE MANUFACTURER SHALL HAVE A REPRESENTATIVE ON THE JOB SITE AT ALL TIMES DURING APPLICATION AND CURE TIME. THE REPRESENTATIVE, ALONG WITH CONSULTATION WITH ENGINEER, MAY SUSPEND ANY ITEM OF WORK THAT IS SUSPECT AND DOES NOT MEET THE REQUIREMENTS OF THE SPECIFICATIONS. WORK SHALL NOT RESUME UNTIL THE ENGINEER AND REPRESENTATIVE ARE SATISFIED THAT THE APPROPRIATE REMEDIAL ACTION HAS BEEN TAKEN BY THE CONTRACTOR.

ALL COSTS FOR AGGREGATE, EPOXY FOR MINIMUM OF TWO LIFTS, SURFACE PREPARATION, LABOR AND ANY OTHER MISCELLANEOUS MATERIALS REQUIRED TO PLACE THIN OVERLAY SHALL BE INCLUDED IN ITEM NO. 617-04.01, TYPE I THIN OVERLAY (EPOXY-URETHANE), S.Y., AS CALLED FOR ON THE QUANTITY SHEET.

THICKNESS VERIFICATION: THE PROJECT ENGINEER SHALL BE NOTIFIED OF THE NUMBER OF GALLONS USED ON THE PROJECT WITH NOTARIZED QUANTITY STATEMENTS FROM THE CONTRACTOR AND THE MANUFACTURER. THE CONTRACTOR SHALL VERIFY TO TDOT THAT THE OVERLAY IS AN AVERAGE OF AT LEAST 3/8" INCH THICK AT THREE RANDOM LOCATIONS AGREED UPON BY THE PROJECT ENGINEER AND THE MATERIAL MANUFACTURER REPRESENTATIVE. IF 3/8 INCH THICK AVERAGE IS NOT ACHIEVED, A RE-TEST SHALL BE PERFORMED IN ADJOINING AREAS. THIN AREAS SHALL BE RE-COATED AS DESCRIBED ABOVE BY THE CONTRACTOR AND REVERIFIED AT NO ADDITIONAL COST TO TDOT. THIS VERIFICATION MAY CONSIST OF CORES, HOLES, ETC., BUT IN ALL CASES ANY DESTRUCTIVELY TESTED AREAS SHALL BE REPAIRED BY THE CONTRACTOR BEFORE FINAL ACCEPTANCE BY THE PROJECT ENGINEER. SEE MANUFACTURER'S RECOMMENDATIONS FOR REQUIRED AMBIENT AND SURFACE TEMPERATURES AND HUMIDITY LIMITS FOR APPLICATION.

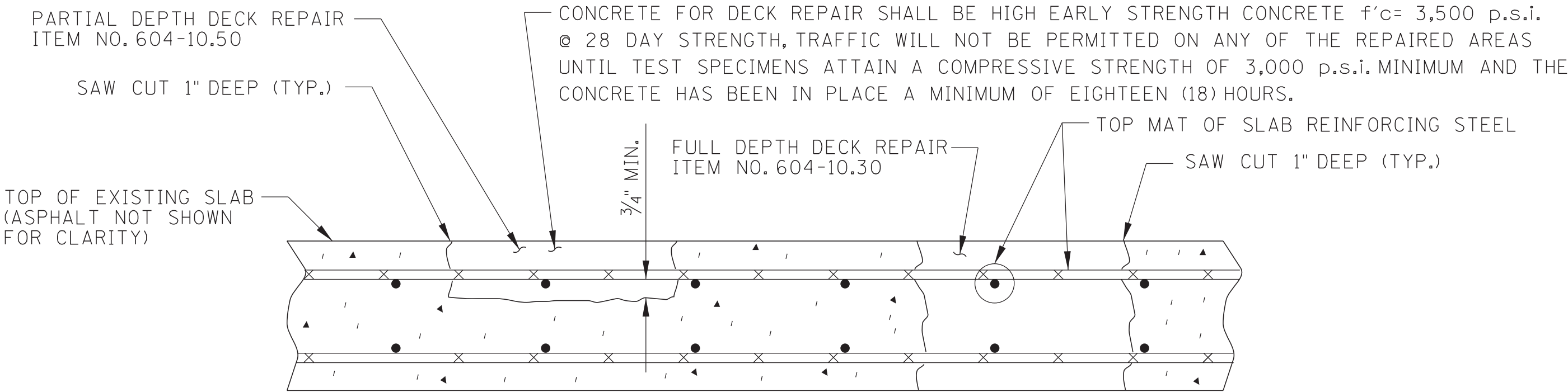
PIN NO.:	134893.00	
DESIGN BY:	R. PARK	DATE: 07/2025
DRAWN BY:	L. MENDEZ	DATE: 07/2025
SUPERVISED BY:	D. MCCRARY	DATE: 07/2025
CHECKED BY:	D. MCCRARY	DATE: 07/2025



CONST. NO.:

PROJECT NO.		YEAR	SHEET NO.
19S045-M3-005		2025	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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CONTRACTOR SHALL TAKE EXTREME CAUTION TO AVOID DAMAGING STAY IN PLACE DECK FORMS



DETAIL SHOWING FULL AND PARTIAL DEPTH DECK REPAIR

REMOVE CONCRETE IN ALL DELAMINATED AREAS TO A DEPTH OF 3/4" BELOW THE TOP BAR OF THE TOP MAT OF REINFORCING STEEL. ALL REINFORCING STEEL IN AREAS OF DECK REPAIR SHALL BE COMPLETELY CLEANED. AREAS OF CONCRETE REMOVAL SHALL BE DESIGNATED BY PERSONNEL FROM THE BRIDGE REPAIR OFFICE. INSPECTIONS TO DETERMINE AREAS OF DECK REPAIR SHALL BE SCHEDULED WITH THE BRIDGE REPAIR OFFICE AT LEAST THREE (3) DAYS IN ADVANCE. DECK REPAIR WILL BE PAID FOR UNDER ITEM NO. 604-10.50, BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB) OR ITEM NO. 604-10.30, BRIDGE DECK REPAIR (FULL DEPTH OF SLAB). DURING PARTIAL DEPTH REPAIRS, SHOULD DETERIORATED CONCRETE BE ENCOUNTERED WHICH APPEARS TO RUN FULL DEPTH IN THE SLAB, THE ENGINEER MAY DESIGNATE THESE AREAS TO BE REPAIRED UNDER ITEM NO. 604-10.30. POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE IN MAKING PARTIAL AND FULL DEPTH REPAIRS ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:

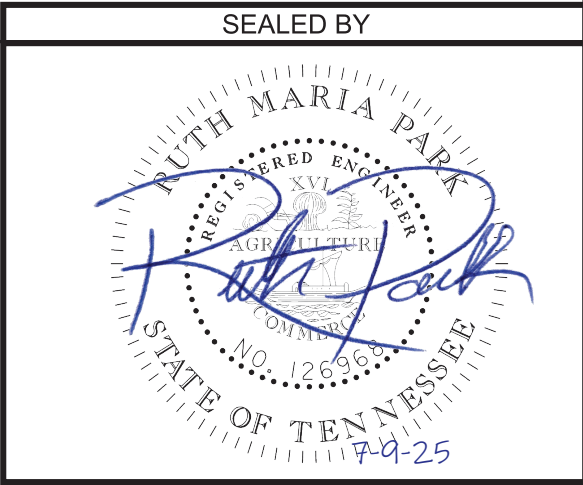
1. (PARTIAL DEPTH REPAIRS) PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 60 POUND CLASS SHALL NOT BE USED.
2. (FULL DEPTH REPAIRS) PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 90 POUND CLASS SHALL NOT BE USED. ALSO ALL DECK REPAIR OVER BEAMS WILL BE RESTRICTED TO 60 POUND PNEUMATIC HAMMERS.
3. CHIPPING HAMMERS OF THE 15 POUND CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.

THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD ALL EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE OF ITEMS BID ON.

WHEN REMOVING CONCRETE FROM THE LIMITS SHOWN, CARE SHALL BE TAKEN NOT TO DAMAGE EXISTING TRANSVERSE REINFORCING STEEL. EXISTING REINFORCING STEEL THAT IS EXPOSED DURING REMOVAL OF PORTIONS OF CONCRETE SLAB SHALL BE THOROUGHLY CLEANED AND MAINTAINED.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS. EXTENSIVE AREAS OF DECK REMOVAL WILL REQUIRE A SUPPORT SYSTEM FOR THE BEAM AND DETAILS SUBMITTED TO THE ENGINEER FOR APPROVAL.

BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB) AND BRIDGE DECK REPAIR (FULL DEPTH OF SLAB) MAY BE INCREASED, DECREASED, OR ELIMINATED AS DIRECTED BY THE ENGINEER.

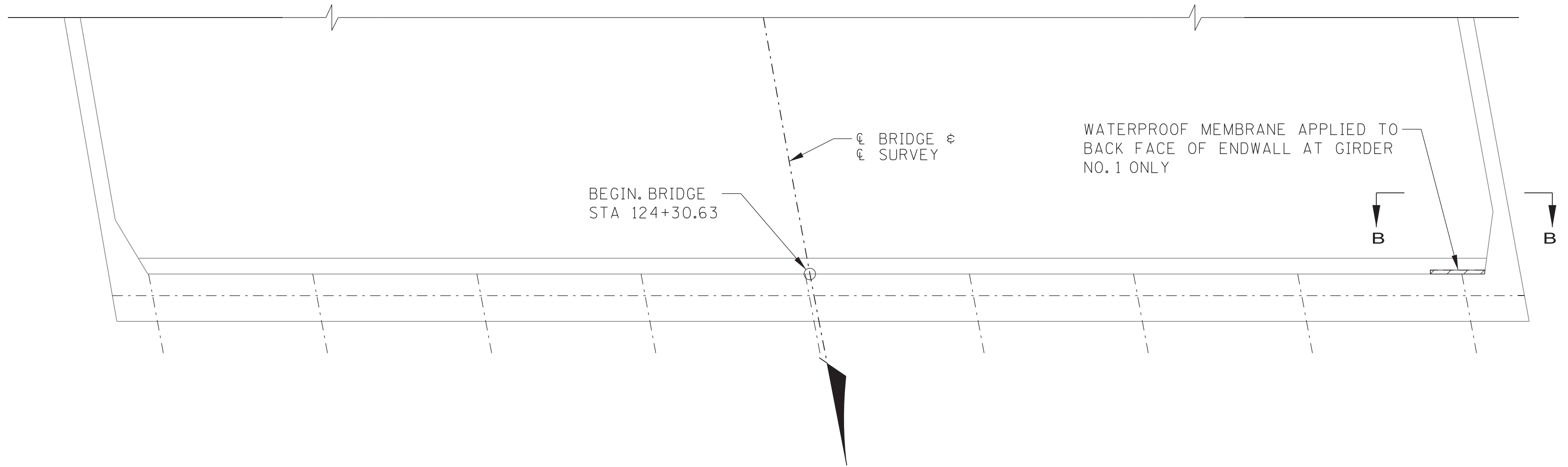


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
MISCELLANEOUS REPAIR
DETAILS
OLD HICKORY BOULEVARD
OVER INTERSTATE 40 EAST
BR. NO. 19-SR045-17.92
FED. ID. NO. 19I00400141
DAVIDSON COUNTY
2025

BR-132-847

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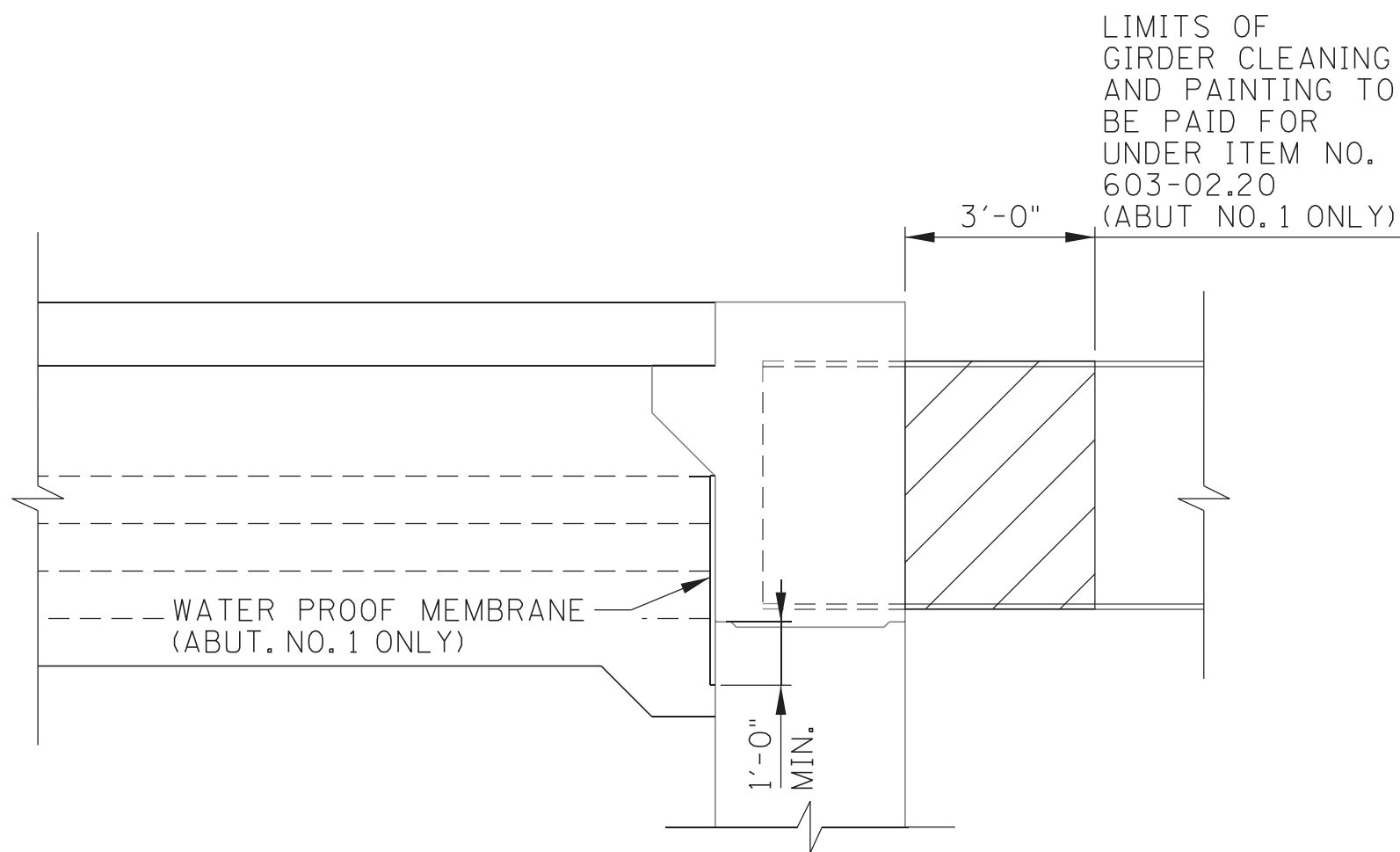
PIN NO.:	134893.00	
DESIGN BY:	R. PARK	DATE: 07/2025
DRAWN BY:	L. MENDEZ	DATE: 07/2025
SUPERVISED BY:	D. MCCRARY	DATE: 07/2025
CHECKED BY:	D. MCCRARY	DATE: 07/2025



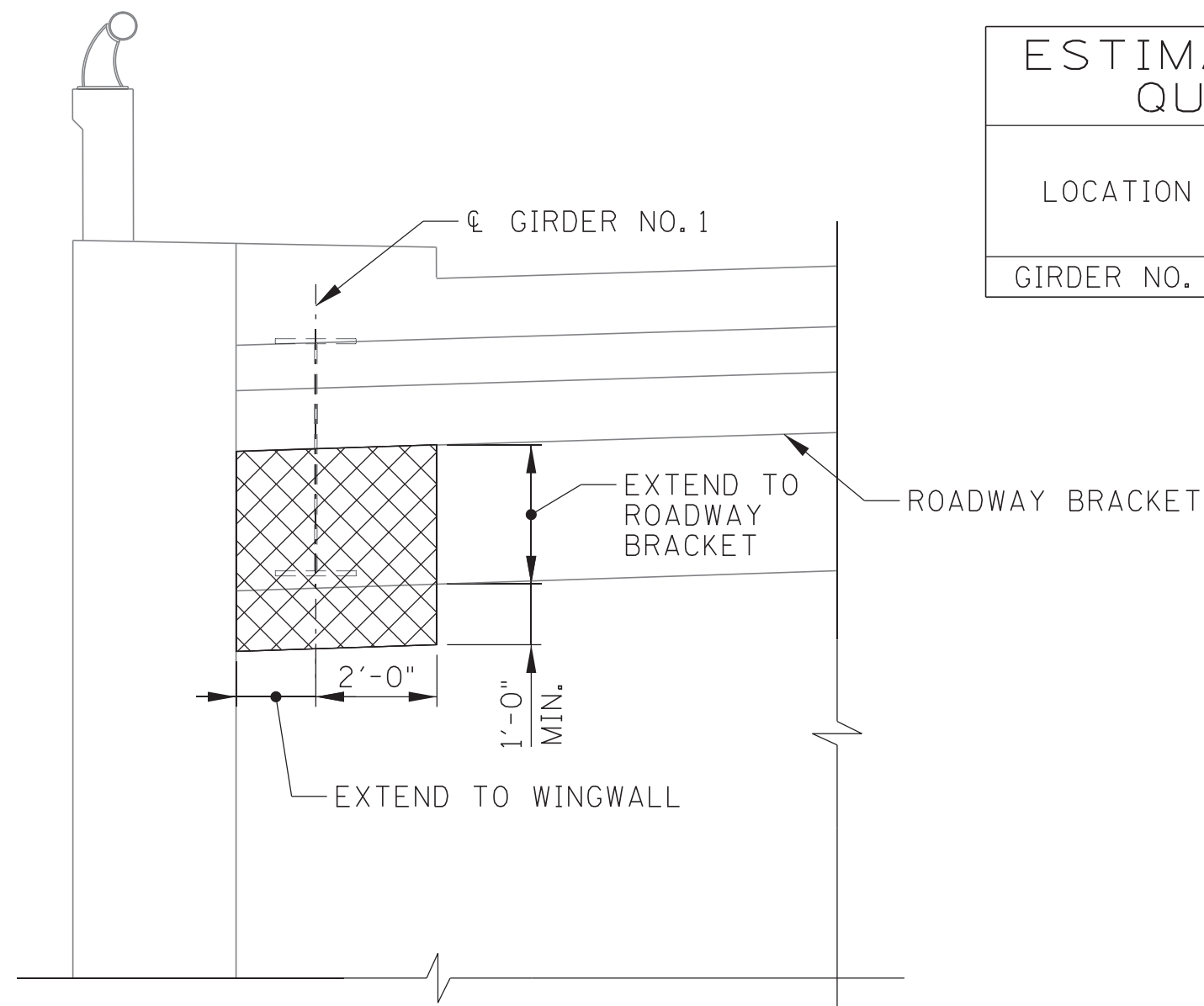
ABUTMENT NO.1 - PLAN



ABUTMENT NO.1 - ELEVATION
(LOOKING BACK ON SURVEY)



SECTION A-A



☒ DENOTES: WATER PROOF MEMBRANE TO BE PLACED BEHIND GIRDER ON BACKSIDE OF THE ENDWALL

SECTION B-B
(ABUTMENT NO.1 ONLY)

CONST. NO.:

PROJECT NO.		YEAR	SHEET NO.
19S045-M3-005		2025	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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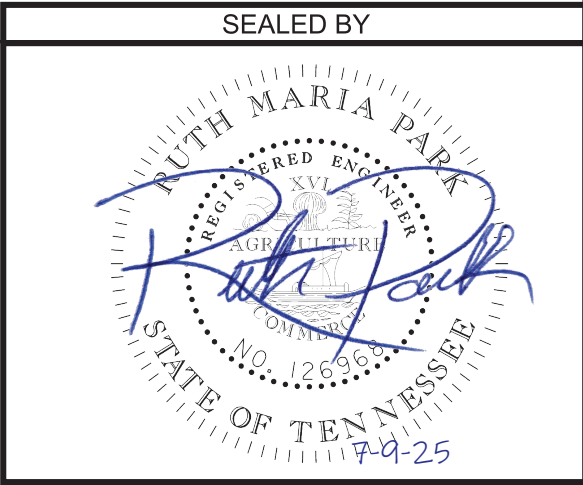
NOTES:

ALL EXISTING STRUCTURAL STEEL WITHIN THE LIMITS OF PAINTING SHOWN SHALL BE PAINTED IN ACCORDANCE WITH 603.03. SCHEDULE OF PAINTING SYSTEM B, OF THE TENNESSEE D.O.T. STANDARD SPECIFICATIONS. COLOR OF THE TOP COAT SHALL COMPLY WITH FEDERAL STANDARD NO. 30059, BROWN. SEE SECTIONS 603 AND 910 OF THE STANDARD SPECIFICATIONS. COST OF SPOT PAINTING TO BE INCLUDED IN ITEM NO. 603-02.20, SPOT PAINTING EXISTING STEEL STRUCTURES, S.F. THIS ITEM WILL ALSO INCLUDE THE COST OF CLEANING EXISTING STRUCTURAL STEEL FOR THE LIMITS SHOWN.

GIRDER END (FLANGES AND WEB) OF EXISTING GIRDER NO.1 SHALL BE CLEANED AND PAINTED ACCORDING TO DETAILS SHOWN ON THIS SHEET.

- CONTRACTOR TO VERIFY ALL LOCATIONS NEEDING CLEANING AND REPAINTING WITH THE ENGINEER PRIOR TO COMMENCING WORK.
- CLEANING OF STEEL SHALL BE DONE WITH A VACUUM-SHROUDED DEVICE CAPABLE OF CONTAINING ALL WASTE MATERIAL.
- WATERPROOF MEMBRANE SHALL BE FROM THE QUALIFIED PRODUCTS LIST 2.

ESTIMATED REPAIR QUANTITIES	
LOCATION	ITEM NO. 603-02.20 SPOT PAINTING EXISTING STEEL STRUCTURES (S.F.)
GIRDER NO.1	35



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
ABUTMENT
REPAIR DETAILS
OLD HICKORY BOULEVARD
OVER INTERSTATE 40 EAST
BR. NO. 19-SR045-17.92
FED. ID. NO. 19I00400141
DAVIDSON COUNTY
2025

PIN NO.:

134893.00

DESIGN BY:

R. PARK

DATE:

07/2025

DRAWN BY:

L. MENDEZ

DATE:

07/2025

SUPERVISED BY:

D. MCCRARY

DATE:

07/2025

CHECKED BY:

D. MCCRARY

DATE:

07/2025



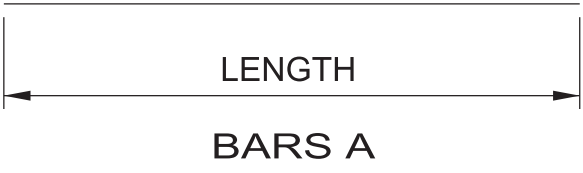
BILL OF STEEL (SIDEWALK)								
BAR	LOCATION	SIZE	NO REQ'D.	BENDING DIMENSION				LENGTH
				A	B	C	D	
A500E	SIDEWALK	5	24	23'-8"				23'-8"
A501E	SIDEWALK	5	100	4'-11 1/2"				5'-0"

REINFORCING STEEL CODE

TYPE	SIZE	SERIES
A	5	06

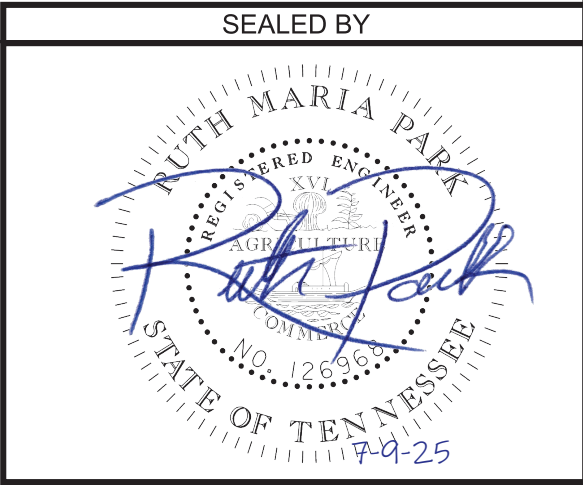
NOTE: DIMENSIONS SHOWN ON THIS SHEET ARE
OUTSIDE TO OUTSIDE OF BAR. STANDARD
C.R.S.I. HOOK DETAILS SHALL APPLY.

NOTE: THE SUFFIX "E", FOR BARS SO MARKED,
DENOTES EPOXY COATED REINFORCEMENT.



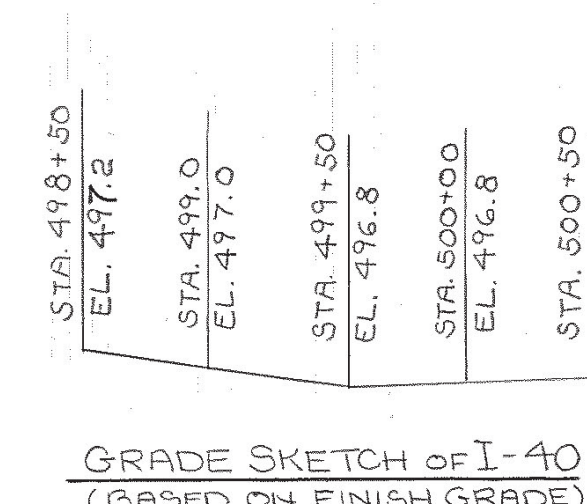
CONST. NO.:

PROJECT NO.		YEAR	SHEET NO.
19S045-M3-005		2025	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BILL OF STEEL

OLD HICKORY BOULEVARD
OVER INTERSTATE 40 EAST
BR. NO. 19-SR045-17.92
FED. ID. NO. 19I00400141
DAVIDSON COUNTY
2025



TITLE	DRAWING NO.	LATEST REV
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LAYOUT	M-246-71	
GENERAL NOTES AND ESTIMATED		
QUANTITIES	M-246-72	1-28-91
FOUNDATION DATA	M-246-73	
SUPERSTRUCTURE	M-246-74	7-10-91
SUPERSTRUCTURE DETAILS	M-246-75	7-10-91
STRUCTURAL STEEL DETAILS	M-246-76	5-22-91
STRUCTURAL STEEL DETAILS	M-246-77	5-22-91
BRIDGE SCREED	M-246-78	7-10-91
ABUTMENT NO. 1	M-246-79	4-25-91
ABUTMENT NO. 1 DETAILS	M-246-80	
ABUTMENT NO. 1 DETAILS	M-246-81	
ABUTMENT NO. 2	M-246-82	4-25-91
ABUTMENT NO. 2 DETAILS	M-246-83	
ABUTMENT NO. 2 DETAILS	M-246-84	5-22-91
BENT NO. 1	M-246-85	5-22-91
BENT NO. 1	M-246-86	
BILL OF STEEL	M-246-86A	
BILL OF STEEL		

TITLE _____ DRAWING No. _____ LATEST REV. _____

[illegible]

<u>TITLE</u>	<u>DRAWING No.</u>	<u>LATEST REV.</u>
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APPROVAL OF SHOP DRAWINGS	105A	7-21-87
REGARDING CONCRETE STRUCTURES	604	5-14-90
REGARDING STRUCTURAL CONCRETE	604C	8-01-89
REGARDING PRECAST PRESTRESSED		
BRIDGE DECK PANELS	604P	5-08-89
REGARDING RIDEABILITY OF BRIDGE DECKS		
AND ROADWAY APPROACHES	604R	5-14-90
EPOXY COATED REINFORCING STEEL	907A	3-25-8
SECTION 602 - STEEL STRUCTURES	602	5-15-8
PAINTING	603A	11-01-8
MACHINED RIP-RAP	709	5-04-8

* DENOTES STANDARD TO BE PRINTED WITH PLANS.

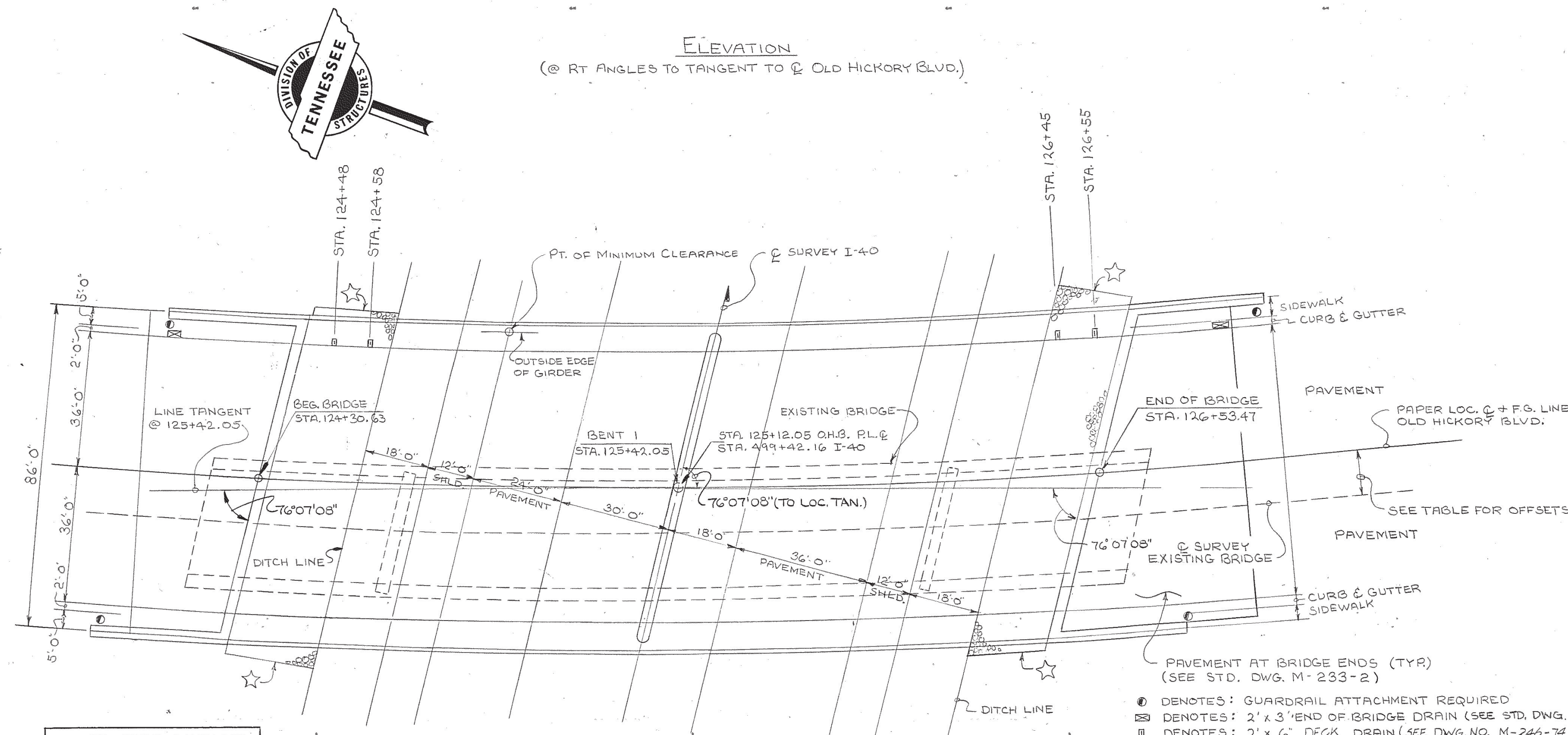
2008 ADT = 18,700 DESIGN SPEED = 40 M.P.H.
76'-0" ROADWAY W/ 5'-0" SIDEWALKS & K-38-162 RAIL.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

LAYOUT
OLD HICKORY BOULEVARD
OVER, INTERSTATE 40 EAST
BRIDGE ID. No. 1910040141
STATION 125+42.00 LOG MI. 29.31
DAVIDSON COUNTY
1990

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

M-246-71



STATION		OFFSET DISTANCE BETWEEN C'S
124+00		12.08'
+25		12.10'
+50		12.11'
+75		12.12'
125+00		12.13'
+25		12.13'
+50		12.14'
+75		12.14'
126+00		12.14'
+25		12.14'
+50		12.14'
+75		12.13'
127+00		12.12'

☆ DENOTES LIMIT OF RIP-RAP
(SEE STD. DWG. RD-SA-1)

P.I. STA. 125 + 92.06
N 647, 778.1644
E 1,822, 615.5462
 $\Delta = 17^{\circ} 33' - 15'$
D = $3^{\circ} 01' 8.28''$
R = 1,847.859'
L = 581.462'
TC = 293.027'
E = 22.488'
SE. = 0.034 Y/
V = 40 M.P.H.
PC. STA. 122 + 99.03
PT. STA. 128 + 80.49

P.I. STA. 489+38.28
 N 647,987.2771
 E 1,821,437.7012
 $\Delta C = 40^{\circ} 49' 03''$
 $D = 1^{\circ} 00' 00''$
 $R = 5729.578'$
 $L = 4081.75'$
 $T = 2131.81'$
 $E_x = 383.74$
 $S.E. = 0.039 \%$
 $V = 55 \text{ M.P.H.}$
 PC. STA. 468+06.47
 PT. STA. 508+88.22

DESIGNED BY D. HARRELL DATE 2-88
 DRAWN BY M. R. MANNCHEN DATE 7-90
 SUPERVISED BY RLH/ HMB DATE 7-90
 CHECKED BY D. HARRELL DATE 10-90

TEMPORARY MEDIAN BARRIER = 275 LIN. FT.

CLASS "A" GRADING "D" = 645 CU. YDS.

GENERAL NOTES

SPECIFICATIONS: STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION. (MARCH 1981 EDITION).

LOADING: HS20-44.

DESIGN SPECIFICATIONS: AASHTO 1989 EDITION WITH ADDENDA AND "GUIDE SPECIFICATIONS FOR SEISMIC DESIGN OF HIGHWAY BRIDGES" 1983 EDITION WITH ADDENDA. (SEISMIC PERFORMANCE CATEGORY A WITH ACCELERATION COEFFICIENT < .075).

CONCRETE: TO BE CLASS "A". f'c 3,000 PSI. UNLESS OTHERWISE NOTED.

CLASS A CONCRETE FOR BRIDGE DECKS SHALL BE IN ACCORDANCE WITH SECTION 604 OF THE STANDARD SPECIFICATIONS EXCEPT AS MODIFIED BY SPECIAL PROVISION 604-C.

BRIDGE DECK SURFACE FINISH: TO BE IN ACCORDANCE WITH NOTE C, SHEET 2, OF SPECIAL PROVISION 604.

BRIDGE DECK FORMS: BRIDGE DECK FORMS FOR CONCRETE DECKS SHALL BE CONSTRUCTED USING EITHER REMOVABLE FORMS OR PERMANENT FORMS. PERMANENT FORMS MAY BE EITHER REMAIN-IN-PLACE STEEL OR PRECAST, PRESTRESSED CONCRETE PANELS. IN EITHER CASE, FORMS SHALL BE ATTACHED BY MEANS OTHER THAN WELDING TO SUPPORT MEMBERS. THE CONTRACTOR SHALL TAKE STEPS TO ASSURE THE STABILITY OF THE EXTERIOR GIRDER AGAINST TWISTING OR OVERTURNING DURING SLAB POURING OPERATIONS.

WHEN THE WIDTH OF THE OVERHANG EXCEEDS THE DEPTH OF THE EXTERIOR GIRDER, DETAILS AND DESIGN CALCULATIONS FOR THE CANTILEVER SUPPORT SYSTEM SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL. IF THE USE OF PERMANENT DECK FORMS REQUIRES ADDITIONAL SLAB THICKNESS, THE CONTRACTOR WILL BE REQUIRED TO REDESIGN THE GIRDERS WHEN THE SLAB THICKNESS IS INCREASED MORE THAN 1 1/2 INCHES. ALL CHANGES TO THE GIRDERS SHALL BE AT THE CONTRACTOR'S EXPENSE.

IF PRECAST PRESTRESSED DECK PANELS ARE USED, THE CONTRACTOR SHALL PROVIDE SUPPLEMENTAL REINFORCING, ADDITIONAL REINFORCING TIES, AND TEMPORARY OR PERMANENT ERECTION DIAPHRAGMS AS REQUIRED BY SPECIAL PROVISION 604P AND STANDARD DRAWING M-164-25A. IT IS THE CONTRACTOR'S RESPONSIBILITY TO INSURE THAT THE PRECAST PANELS WILL FIT BETWEEN THE ERECTED GIRDERS SUCH THAT THE PANEL OVERHANG LIMITS GIVEN ON STANDARD DRAWINGS M-164-24 AND 25 ARE MAINTAINED. PANELS WHICH DO NOT FIT MUST BE REPLACED AT THE CONTRACTOR'S EXPENSE.

ESTIMATED QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	TOTAL	SUPERSTRUCTURE	ABUT. NO. 1	BENT NO. 1	ABUT. NO. 2
② 202-04.01	REMOVAL OF STRUCTURES (19-SR45-17.92)	L.S.	1				
③ 204-02.01	DRY EXCAVATION (BRIDGES)	C.Y.	848		302	116	430
③ 204-04.01	ROCK EXCAVATION (BRIDGES)	C.Y.	8			8	
204-05	ROCK DRILLING (BRIDGES)	L.F.	12			12	
④ 602-03	STEEL STRUCTURES	L.S.	1				
⑥ 604-01.12	CLASS 'A' CONCRETE (BRIDGE DECK)	C.Y.	583	583			
⑪ 604-02.03	EPOXY COATED REINFORCING STEEL	LB.	189,539	154,998	16,080		18,461
⑤ 604-03.01	CLASS 'A' CONCRETE (BRIDGES)	C.Y.	500	90	137	83	190
⑫ 604-03.02	STEEL BAR REINFORCEMENT (BRIDGES)	LB.	38,618	2,748	7,604	18,802	9,464
① 604-03.04	PAVEMENT AT BRIDGE ENDS	S.Y.	671				
604-04.01	APPLIED TEXTURE FINISH (NEW STRUCTURES)	S.Y.	1,076				
606-02.03	STEEL PILES (10 INCH)	L.F.	1,070				
⑦ ⑩ ⑨ 620-14	CONCRETE PARAPET WITH STRUCTURAL TUBING (K-38-162)	L.F.	574				
709-05.04	MACHINED RIP-RAP (3 IN. TO 6 IN.)	C.Y.	107				
③ 710-09.01	6" PERF. PIPE WITH VERTICAL DRAIN SYSTEM	L.F.	284		141		143
710-09.02	6" PIPE UNDERDRAIN	L.F.	46		23		23
⑪ 714-01.01	STRUCTURE LIGHTING	L.S.	1				

① NOTE: SQUARE YARD FOR PAVEMENT AT BRIDGE ENDS SHALL BE MEASURED AS ROAD SURFACE AREA AND SIDEWALKS AND SHALL INCLUDE ALL CONCRETE, REINFORCING STEEL, PILES, JOINT MATERIAL, SURFACE FINISH AS PER SP604 AND ANY OTHER INCIDENTALS NECESSARY FOR COMPLETE INSTALLATION. PRIOR TO CONSTRUCTION OF THE PAVEMENT AT BRIDGE ENDS, THE CONTRACTOR SHALL SUBMIT A PROPOSED BILL OF STEEL TO THE ENGINEER FOR APPROVAL.

② NOTE: LUMP SUM: COMPLETE REMOVAL OF EXISTING BRIDGE TO INCLUDE FOUR-SPAN CONCRETE SUB ABUTMENTS AND CONCRETE BENTS. TOTAL LENGTH = 249'-0" OUT-TO-OUT WIDTH = 34'-6" BENTS TO BE REMOVED TO 1'-0" BELOW FINAL GROUND LINE OR AS REQUIRED TO AVOID CONFLICT WITH NEW CONSTRUCTION.

③ EXCAVATION BASED ON FINAL PROFILE.

④ NOTE: LUMP SUM: TOTAL ESTIMATED WEIGHT OF 485,841 LBS. OF STRUCTURAL STEEL INCLUDES (BEARING PLATE AT BENT, SHEAR CONNECTORS, BOLTS, RIVETS, ETC.) ALSO SEE TENNESSEE STANDARD SPECIFICATIONS SECTION 602.53 AND 602.54.

A325 = 6,857 LB.
A36 = 65,183 LB.
A572 = 431,801 LB.

DESIGNED BY: DAVID HARRELL DATE: 6-90
DRAWN BY: MARK DYE DATE: 8-90
SUPERVISED BY: HMB/RLH DATE: 8-90
CHECKED BY: DAVID HARRELL DATE: 10-90

REINFORCING STEEL: TO BE ASTM A615 GRADE 60. STANDARD CRSI HOOK DETAILS APPLY UNLESS OTHERWISE NOTED ON BILL OF STEEL. SPACING DIMENSIONS ARE CENTER TO CENTER AND COVER DIMENSIONS ARE CLEAR DISTANCE UNLESS OTHERWISE NOTED. PLACING TOLERANCES ARE ± 1/4" FOR SPACING AND - 1/8" OR + 3/8" FOR COVER. THE SUFFIX E, FOR BARS SO MARKED, DENOTES EPOXY COATED REINFORCEMENT. SEE SPECIAL PROVISION 907A.

SPECIAL NOTE: FOUNDATIONS FOR BENTS SHALL BE EXCAVATED TO THE BOTTOM OF FOOTING ELEVATIONS SHOWN; ROD SOUNDINGS SHALL THEN BE MADE AS DIRECTED BY THE ENGINEER. FROM THE RESULTS OBTAINED THE ENGINEER WILL DECIDE IF PILES WILL BE USED OR THE FOOTINGS CARRIED TO ROCK. COST OF ROD SOUNDINGS TO BE INCLUDED IN ITEMS BID ON. NO REINFORCING STEEL FOR BENT COLUMNS SHALL BE ORDERED UNTIL FINAL ELEVATIONS HAVE BEEN DETERMINED.

PILES: TO BE HP 10 X 42 DRIVEN TO REFUSAL ON ROCK OR A MINIMUM BEARING OF 55 TONS FOR THE BENTS AND 55 TONS FOR THE ABUTMENTS.

SHOP DRAWINGS: SEE SPECIAL PROVISION NO. 105A.

WELDING: SEE SPECIAL PROVISION NO. 602 AND NOTES ON DRAWING NO. M-246-74.

PAINT: SYSTEM B - INORGANIC ZINC - URETHANE FINISH BROWN TOP COAT - SEE TENNESSEE STANDARD SPECIFICATION 603.05(b) AND SPECIAL PROVISION 603A.

PAINTING STRUCTURAL STEEL: ALL SURFACES OF STRUCTURAL STEEL GIRDERS MUST BE PAINTED INCLUDING THE TOP OF THE TOP FLANGE, SPLICES AND BEARING AREAS. HOWEVER, THE FAYING SURFACES OF FIELD SPLICES SHALL RECEIVE ONLY 1 MIL THICKNESS OF SHOP COAT.

RADIOGRAPHIC, ULTRASONIC, AND MAGNETIC INSPECTION: SEE SPECIAL PROVISION NO. 602. NOTES ON DRAWING NO. M-246-74.

STEEL STRUCTURES: SEE TENNESSEE STANDARD SPECIFICATIONS SECTION 602 AND NOTES ON DRAWING NO. M-246-74.

SPECIAL NOTE: FOOTING FOR BENTS - AFTER EXCAVATION TO ROCK FOR PHASE II FOOTINGS HAVE BEEN COMPLETED, HOLES 6" DEEP SHALL BE DRILLED AT POINTS DESIGNATED BY THE ENGINEER. FROM THE RESULTS OBTAINED, THE ENGINEER SHALL DETERMINE THE FINAL FOOTING ELEVATIONS. NO REINFORCING STEEL FOR BENT COLUMNS SHALL BE ORDERED UNTIL FINAL FOOTING ELEVATIONS HAVE BEEN DETERMINED.

⑤ NOTE: COST OF RUBBER BONDING CEMENT, ELASTOMERIC BEARING PADS, AND ANCHOR BOLTS TO BE INCLUDED IN UNIT PRICE BID FOR CLASS "A" CONCRETE.

⑥ NOTE: COST OF 4 BRIDGE DECK DRAINS TO BE INCLUDED IN THE UNIT PRICE BID FOR CLASS 'A' CONCRETE (BRIDGE DECK).

⑦ NOTE: COST OF BRIDGE RAIL ENDPOST IS TO BE INCLUDED IN THE COST OF THE BRIDGE RAIL SYSTEM.

⑧ NOTE: COST OF POLYETHYLENE SHEETING AND ALL MISCELLANEOUS ITEMS NECESSARY FOR INSTALLATION TO BE INCLUDED IN COST OF PERFORATED PIPE.

⑨ NOTE: THE COST OF 8 INSERT ASSEMBLIES AND 3 7/8" Ø X 4" HEX HEAD BOLTS, (A307), TO BE INCLUDED IN ITEM 620-14.

⑩ ALL REINFORCING STEEL INCLUDED IN PAY ITEM TO BE EPOXY COATED. COST TO BE INCLUDED IN THE PRICE BID FOR ITEM 620-14.

⑪ NOTE: ITEM INCLUDES 590 L.F. OF 2" Ø CONDUIT AND 4 PULL BOXES.

MACHINED RIP-RAP: MACHINED RIP-RAP FOR SLOPE PROTECTION SHALL BE 3" TO 6" IN SIZE UNIFORMLY GRADED AND MEET THE QUALITY REQUIREMENT OF SUBSECTION 918-10 AND PAID FOR UNDER ITEM 709-05.04 - C.Y.

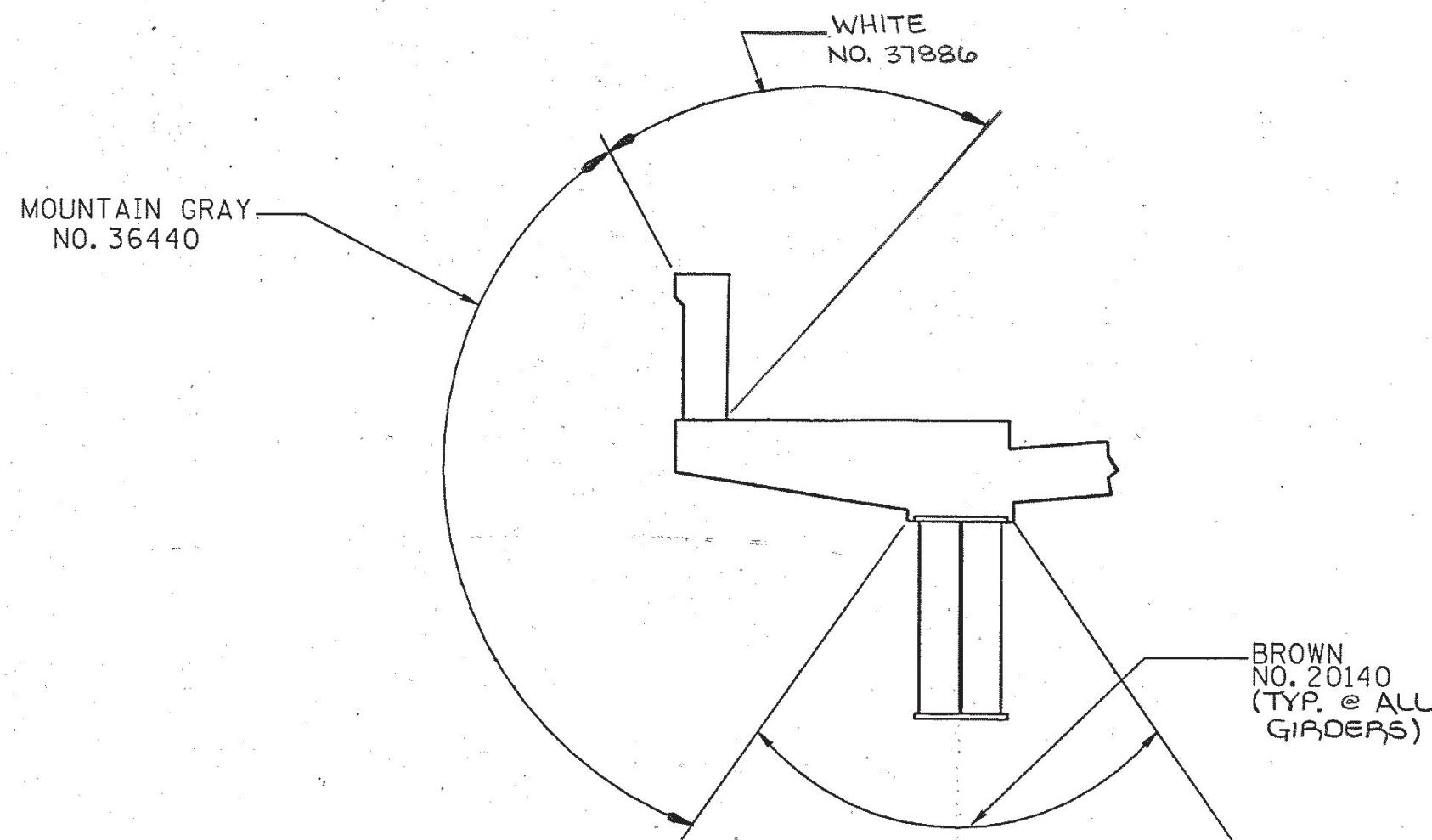
NON-PAY ITEMS: ONLY ITEMS SHOWN ON THE PROPOSAL AS PAY ITEMS WILL BE PAID FOR. COMPENSATION FOR ALL LABOR, MATERIALS, TOOLS, EQUIPMENT, AND INCIDENTALS FOR THE ENTIRE CONTRACT SHALL BE INCLUDED IN THE PRICE BID FOR PAY ITEMS.

FINISHING CONCRETE SURFACES: CONCRETE FINISHING SHALL BE IN ACCORDANCE WITH SECTION 604.22 OF THE TENNESSEE STANDARD SPECIFICATION. AN APPLIED TEXTURE FINISH SHALL BE USED IN LIEU OF A CLASS II FINISH. THE COLOR OF THE FINISH SHALL BE SIMILAR TO MOUNTAIN GREY, FEDERAL SPECIFICATION NO. 36440, FEDERAL COLOR STANDARD NO. 595G, AND A COLOR SAMPLE SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL. NO TEXTURE FINISH SHALL BE APPLIED PRIOR TO COMPLETION OF PAVING AND HAULING OPERATIONS AT THE BRIDGE SITE. PAYMENT FOR THE APPLIED TEXTURE FINISH SHALL BE UNDER ITEM 604-04.01.

REQUIREMENTS AND RESTRICTIONS FOR PHASE CONSTRUCTION

- 1 - THE STAGE CONSTRUCTION SEQUENCE MAY PROHIBIT THE EXTRACTION OF SOME COFFERDAM SHEET PILING. ALL COSTS ASSOCIATED WITH SHEET PILING SHALL BE INCLUDED IN THE LUMP SUM BID PRICE FOR FOUNDATION PREPARATION.
- 2 - THE LOCATION OF LONGITUDINAL CONSTRUCTION JOINTS SHALL NOT BE CHANGED TO ACCOMMODATE REMAIN IN PLACE DECK FORMS.
- 3 - NO SHEET PILES OR BEARING PILES MAY BE DRIVEN FROM THE EXISTING OR PROPOSED STRUCTURE.
- 4 - TWO-12 FOOT TRAFFIC LANES SHALL BE MAINTAINED AT ALL TIMES.

NOTE: THE CONTRACTOR SHALL SUPPORT THE ABUTMENTS UNTIL THE SUPERSTRUCTURE IS IN PLACE, FALSEWORK HAS BEEN REMOVED AND BACKFILLING HAS BEEN COMPLETED.



APPLIED TEXTURE FINISH SKETCH

NOTE: IN ADDITION TO THE PORTIONS SHOWN IN THE SURFACE FINISH SKETCH, THE FOLLOWING EXPOSED CONCRETE IS TO RECEIVE AN APPLIED TEXTURE FINISH: ABUTMENT BEAM AND WINGS AND ENTIRE PIER TO FINISHED GROUND LINE.

⑫ MECHANICAL BAR SPLICES MUST BE APPROVED BY THE DIVISION OF MATERIALS AND TESTS. DOWEL BAR SPICER AND DOWEL-IN SHALL BE DEFORMED, GRADE 60. WHEN EPOXY COATING IS REQUIRED, THE EXPOSED THREADS SHALL BE REPAIRED AFTER SPLICING ACCORDING TO SPECIAL PROVISION 907A. THE SPLICING BAR AND ROOT DIMENSION AT THE THREAD SHALL BE NO LESS THAN THE NOMINAL DIAMETER OF THE SIZE BAR DESIGNATED TO BE SPLICED. THE COST OF FURNISHING THE BAR SPLICES AND DOWELS (EPOXY COATED WHEN REQUIRED) INCLUDING ALL LABOR AND MATERIALS NECESSARY FOR COMPLETE INSTALLATION SHALL BE INCLUDED IN THE PRICE, PER POUND, OF ITEM 604-03.02 AND ITEM 604-02.03.

⑬ NOTE: REMOVAL OF EXISTING SUPERSTRUCTURE IS TO BE ACCOMPLISHED BY SAW CUTTING SLAB AND BEAMS INTO SECTIONS AND REMOVING THOSE SECTIONS IN SUCH A MANNER THAT WOULD INSURE THAT NO DEBRIS IS DROPPED ON THE INTERSTATE BELOW. SEE NOTES IN ROADWAY PLANS FOR MAINTAINING TRAFFIC DURING REMOVAL PROCESS.

CONST. NO. 19008-3165-44

PROJECT NO.	YEAR	SHEET NO.
IR-40-5(106)221	1990	

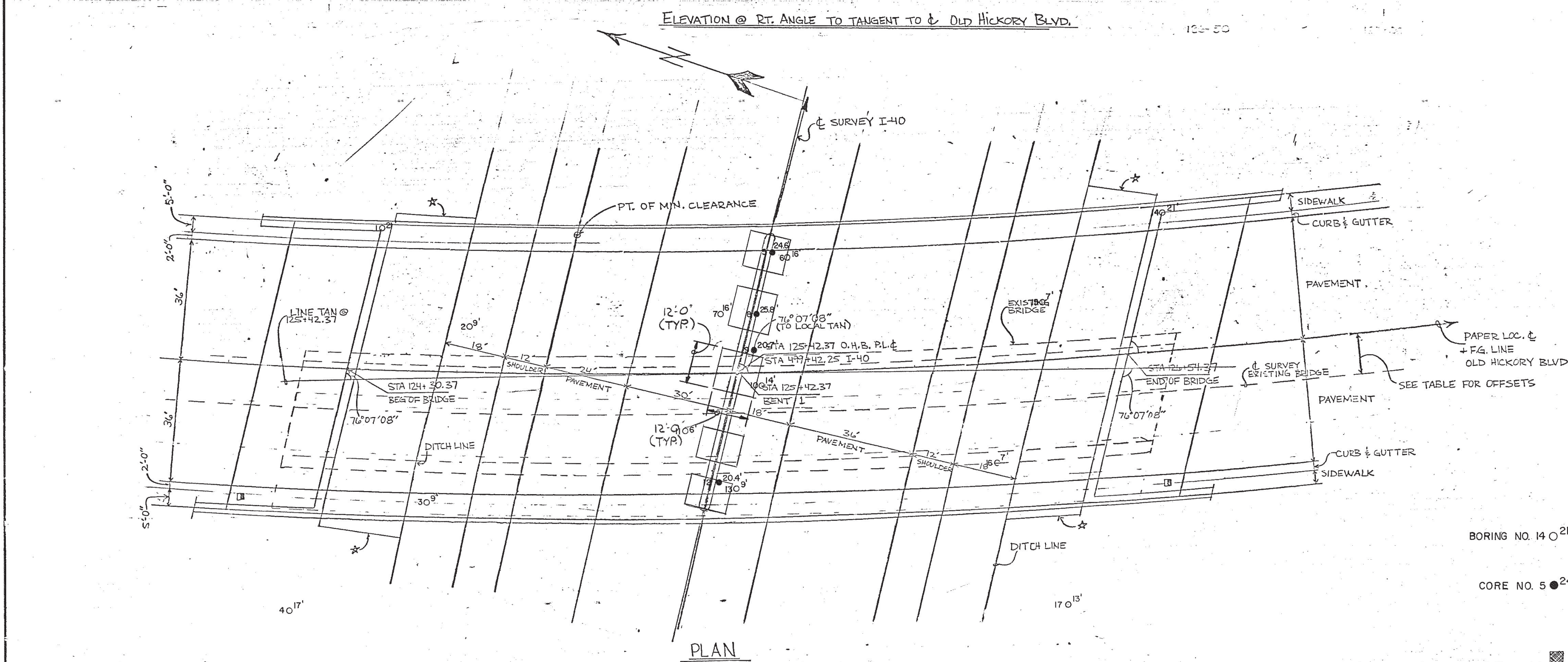
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	1-28-91	DTH	ADDED UTILITY NOTE AND REVISED APPLIED TEXTURE FINISH SKETCH.

⚠ SPECIAL NOTE FOR UTILITIES : IT IS INTENDED THAT THE COST OF MATERIALS AND LABOR NECESSARY FOR THE COMPLETE INSTALLATION OF UTILITIES SHALL BE BORNE BY OTHERS AND SHALL NOT BE PAID FOR AS A PART OF THIS CONTRACT. THE CONTRACTOR SHALL COOPERATE WITH OTHERS IN THE INSTALLATION OF UTILITIES WITH NO ADDITIONAL COMPENSATION ALLOWED THE CONTRACTOR AS A RESULT.

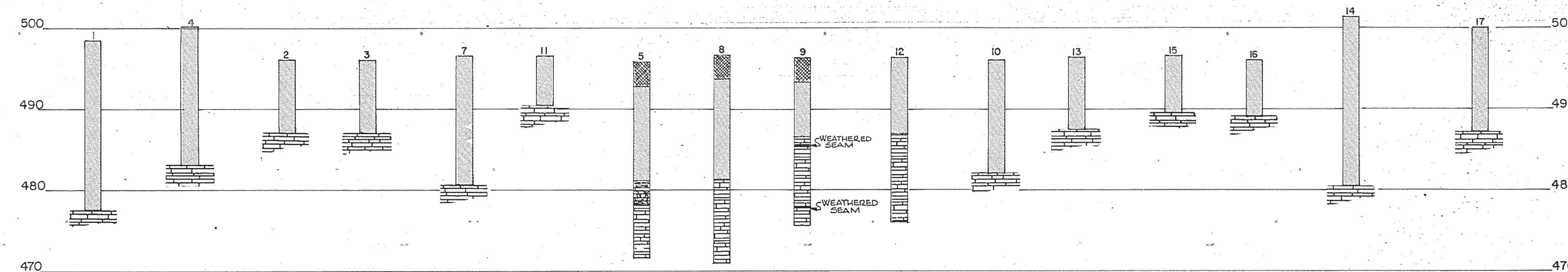
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS






GENERAL NOTES AND
ESTIMATED QUANTITIES
OLD HICKORY BOULEVARD OVER
INTERSTATE 40 EAST
STATION 125+42.00 LOG MILE 29.31
DAVIDSON COUNTY
1990

CORRECT *Edward P. Wasserman*
ENGINEER OF STRUCTURES

[illegible]

CORE NO. 5 ● 24.6' DEPTH DRILLED



 ROAD FILL
 CLAY
 LIMESTONE, FRACTURED
 LIMESTONE, WEATHERED
 LIMESTONE

1. APPROXIMATE EXISTING GROUND LINE AND ROCK LINE.
2. SUFFICIENT GROUND, ROCK, AND CORING INFORMATION FOR BRIDGE FOUNDATIONS.

NOTE: THIS DRAWING IS FOR FOUNDATION INFORMATION ONLY AND IS NOT TO BE USED AS A LAYOUT.

GROUND AND ROCK ELEVATIONS

1	$\frac{6495.4}{A 477.4}$	6	$\frac{A 480.9}{A 476.5}$	11	$\frac{6496.5}{A 470.5}$	16	$\frac{A 496.0}{A 480.0}$
2	$\frac{6496.0}{A 487.0}$	7	$\frac{A 476.5}{A 480.5}$	12	$\frac{A 496.3}{A 487.0}$	17	$\frac{5499.02}{A 487.0}$
3	$\frac{6496.0}{A 487.0}$	8	$\frac{A 496.5}{A 480.9}$	13	$\frac{A 496.3}{A 487.3}$		
4	$\frac{6500.1}{A 483.1}$	9	$\frac{A 496.3 \pm}{A 480.5}$	14	$\frac{6501.4}{A 480.4}$		
5	$\frac{6495.9}{A 481.2}$	10	$\frac{6496.0}{A 482.0}$	15	$\frac{A 496.5}{A 482.5}$		

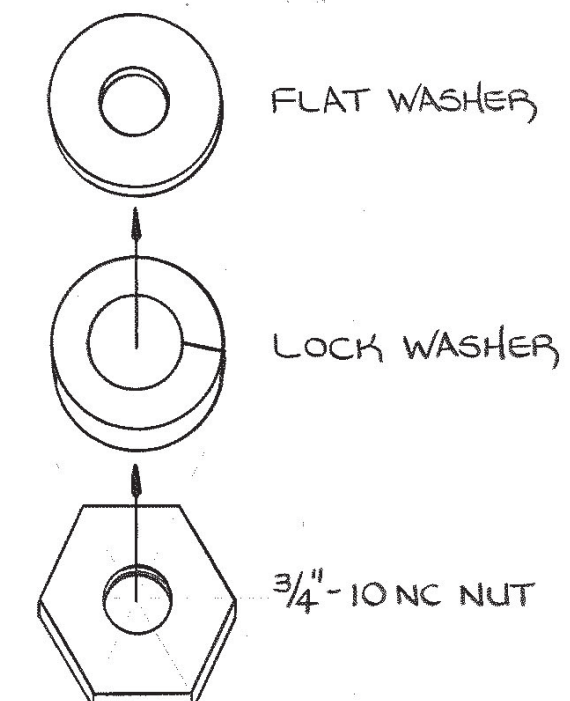
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
FOUNDATION DATA
BRIDGE NO. 1
OLD HICKORY BOULEVARD OVER
INTERSTATE 40 EASTBOUND
STATION 125+42.00
DAVIDSON COUNTY
1990

DESIGNED BY D. HARRELL DATE 2-88
DRAWN BY (REPO)STEVEN STEELE DATE 3-88
SUPERVISED BY D. HARRISON/H. BROOKS DATE 3-88
CHECKED BY D. HARRELL DATE 10-90

CORRECT Edward P. Wasseman
ENGINEER OF STRUCTURES

APPROVED Lewis Evans
DIRECTOR OF HIGHWAYS

M-246-73



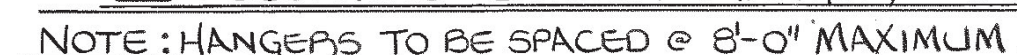
CLASS 'A' CONC. (C.Y.) (BRIDGE DECK)	CLASS 'A' CONC. (BRIDGES) (C.Y.)	STEEL BAR REIN. (LB.)	EPOXY COATED REIN. STEEL (LB.)
583	90	2,748	154,998

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

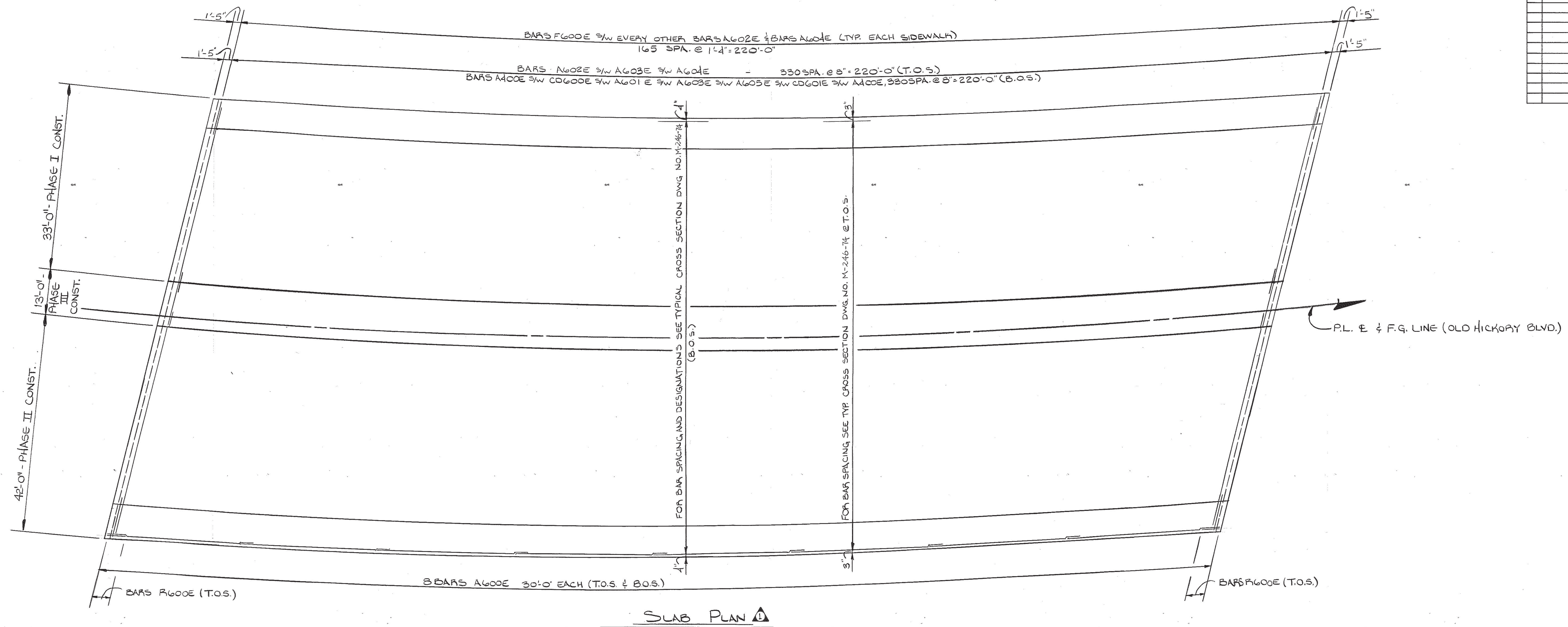
SUPERSTRUCTURE
OLD HICKORY BOULEVARD OVER
INTERSTATE 40 EAST
STATION 125+42.00
LOG MILE 29.31
DAVIDSON COUNTY
1990

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

M-246-74

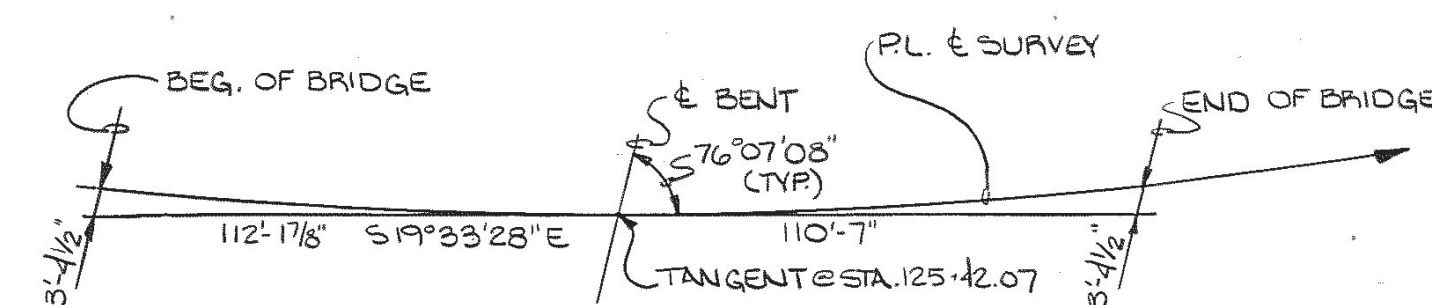


DESIGNED BY DAVID HARRELL DATE 7/90
DRAWN BY D.W. COBLE DATE 8/90
SUPERVISED BY HARRISON-BROOKS DATE 8/90
CHECKED BY DAVID HARRELL DATE 10/90

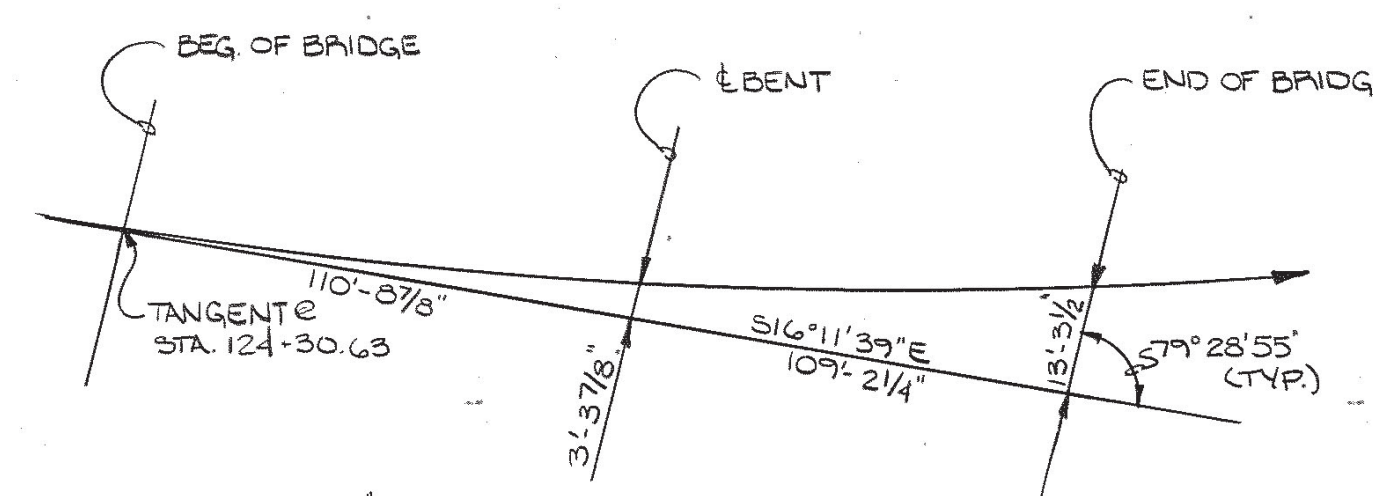
[illegible]

LEGEND :

T.O.S. DENOTES TOP OF SLAB.
B.O.S. DENOTES BOTTOM OF SLAB.



ALTERNATE No.1



ALTERNATE No.2

STAKEOUT PLAN

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
SUPERSTRUCTURE DETAILS
OLD HICKORY BOULEVARD OVER
INTERSTATE 40 EAST
STATION 125+42.00
LOG MILE 29.31
DAVIDSON COUNTY
1990

DESIGNED BY DAVID HARRELL DATE 7/90
DRAWN BY D.W. COBLE DATE 8/90
SUPERVISED BY HARRISON - BROOKS DATE 8/90
CHECKED BY DAVID HARRELL DATE 10/90

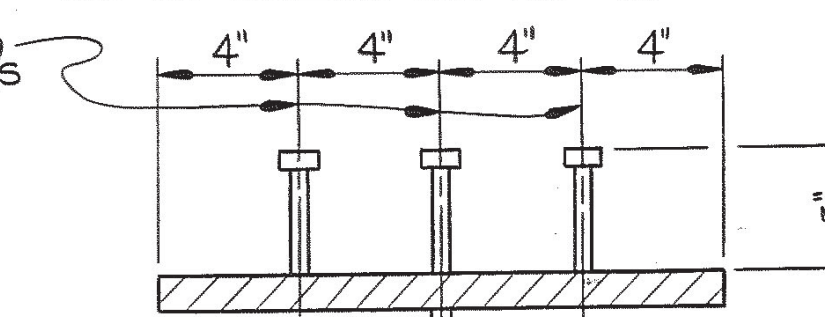
CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

M-246-75

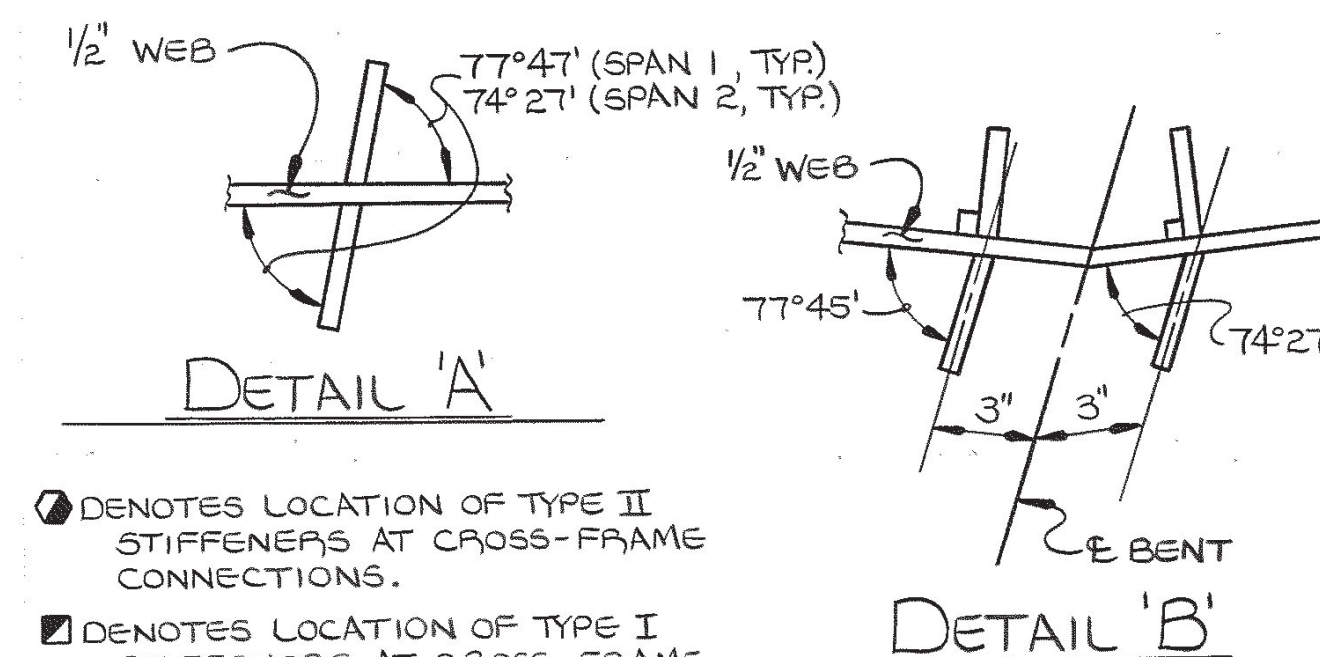
CONST. No. 19008-3165-44

PROJECT NO.	YEAR	SHEET NO.
IA-40-5(10/22)	1990	

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	5/22/91	DTH	ADDED NOTE FOR DETAIL 'C'



SECTION 'C'-C'
(SHOWING SHEAR CONNECTOR SPACING)



DETAIL 'A'
DETAIL 'B'
NOTE: STIFFENERS ARE PERPENDICULAR TO WEB FOR EXTERIOR SIDE OF EXTERIOR GIRDER ONLY.

STEEL BEAM DESIGN DATA (PER BEAM)

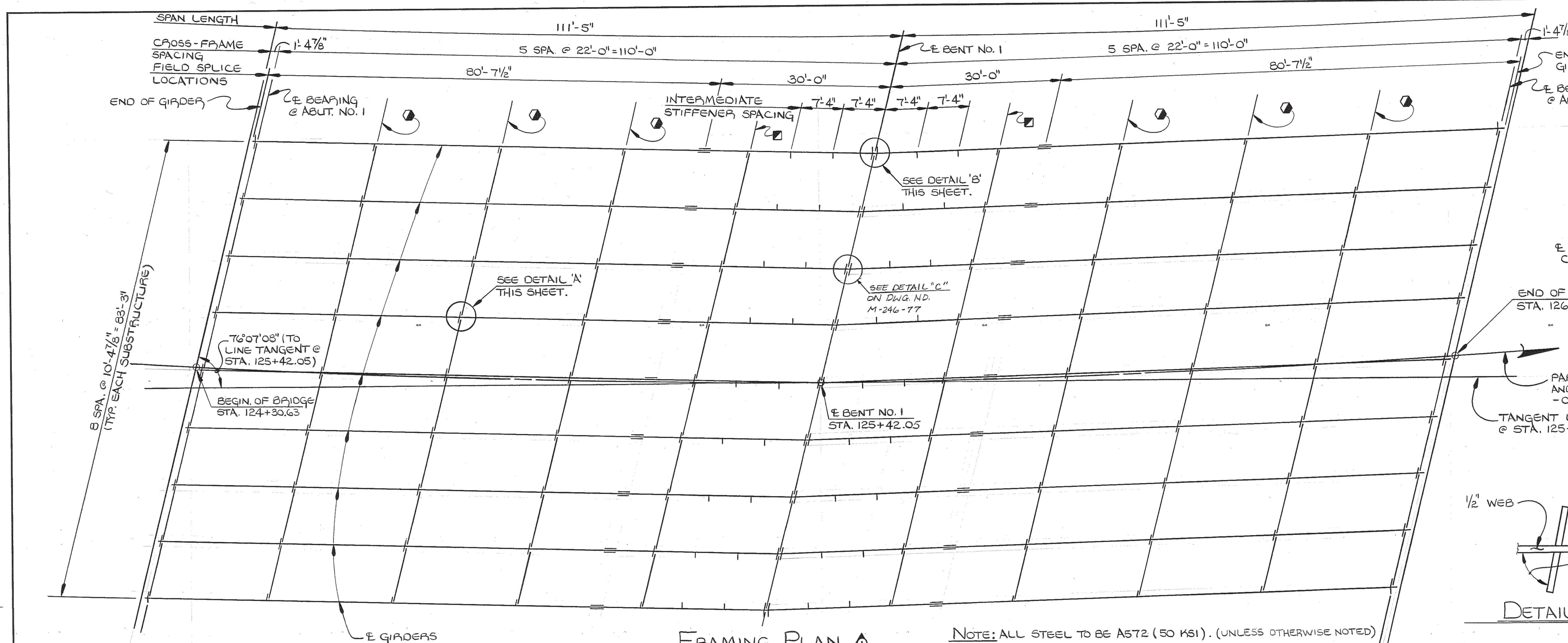
LIVE LOAD DISTRIBUTION FACTOR : 1.564 WHEELS
DEAD LOAD : (EXCLUDING BEAM WT.)
NON-COMPOSITE = 1180 LB./FT.
COMPOSITE = 323 LB./FT.
MAXIMUM POSITIVE MOMENT : (INCLUDING BEAM WT.) (LOAD FACTORED)
D.L. COMPOSITE = 386 K-FT.
D.L. NON-COMPOSITE = 1438 K-FT.
L.L. + I. = 2973 K-FT.
TOTAL = 4797 K-FT.
MAXIMUM NEGATIVE MOMENT : (INCLUDING BEAM WT.) (LOAD FACTORED)
D.L. COMPOSITE = 558 K-FT.
D.L. NON-COMPOSITE = 2866 K-FT.
L.L. + I. = 2249 K-FT.
TOTAL = 5673 K-FT.
COMPOSITE DESIGN SLAB (f'_c = 3000 P.S.I.) : 8 1/2 IN. x 122 IN.
STATE OF TENNESSEE

DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

STRUCTURAL STEEL DETAILS
OLD HICKORY BOULEVARD
OVER INTERSTATE 40 EAST
STATION 125+42.00 L.M. 29.31
DAVIDSON COUNTY
1990

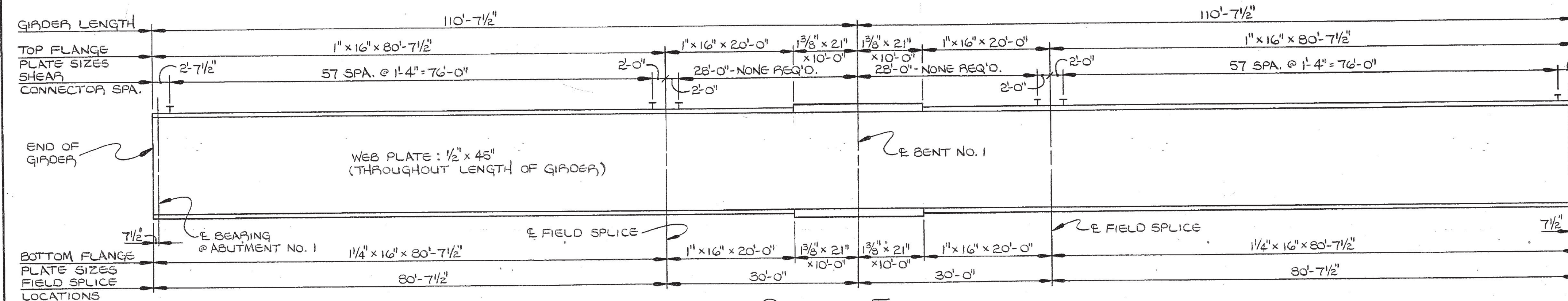
CORRECT *Edward P. Wasserman*
ENGINEER OF STRUCTURES

M-246-76

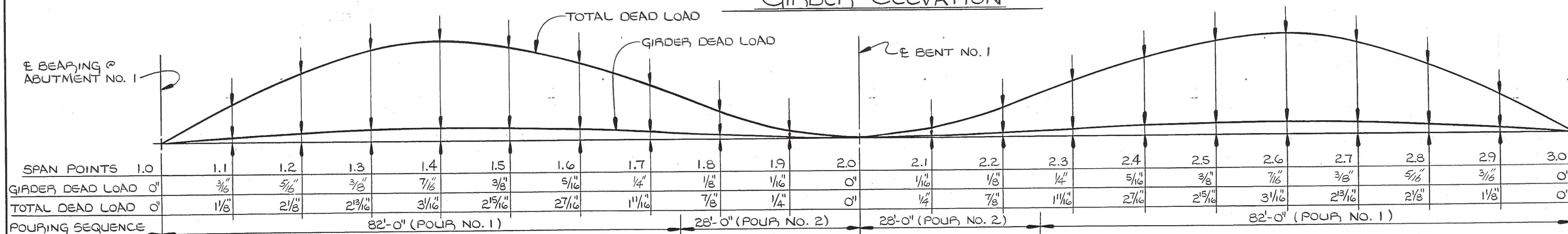


FRAMING PLAN

NOTE: ALL STEEL TO BE A572 (50 KSI). (UNLESS OTHERWISE NOTED)



GIRDER ELEVATION



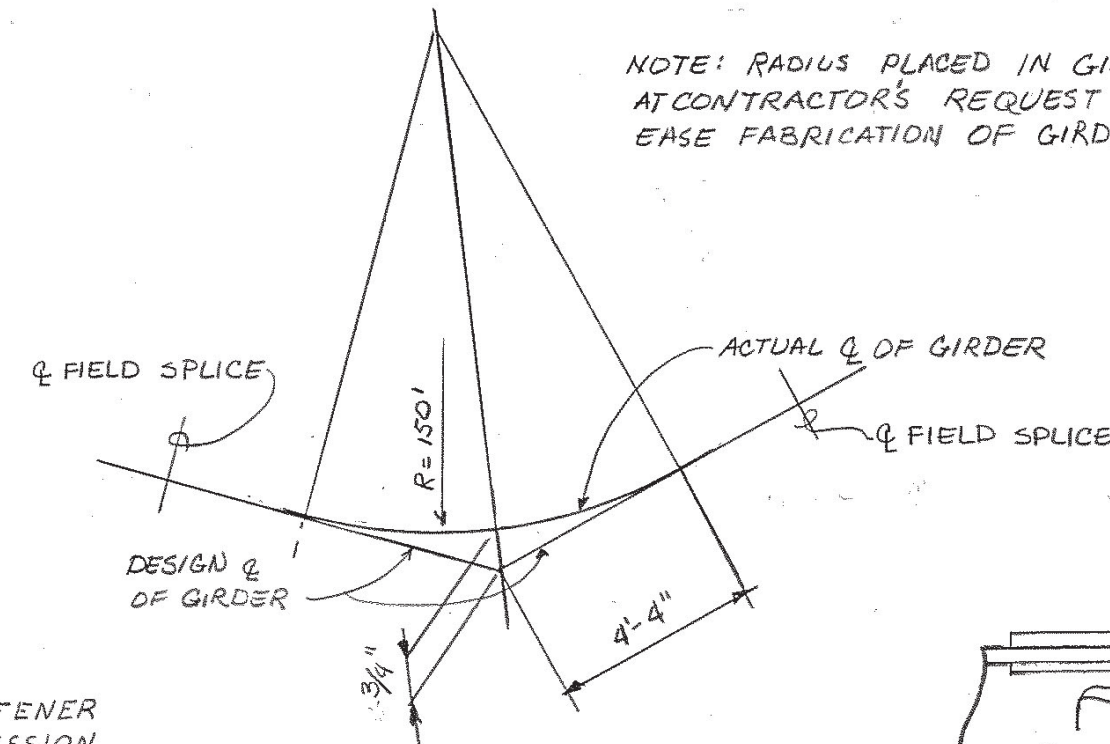
DEAD LOAD CORRECTION CURVES AND POURING SEQUENCE

DEAD LOAD CORRECTION CURVE: GIRDERS SHALL BE CAMBERED TO COMPENSATE FOR DEAD LOAD DEFLECTION AND VERTICAL CURVE. IF PRESTRESSED DECK PANELS ARE USED AND THE BEAMS ARE PROFILED AFTER PANELS ARE IN PLACE, REDUCE THE DEAD LOAD CORRECTION VALUES SHOWN BY 25%.

DESIGNED BY DAVID HARRELL DATE 8-90
DRAWN BY MARY OYE DATE 9-90
SUPERVISED BY R. L. HARRISON & H. M. PROSSER DATE 9-90
CHECKED BY DAVID HARRELL DATE 10-90

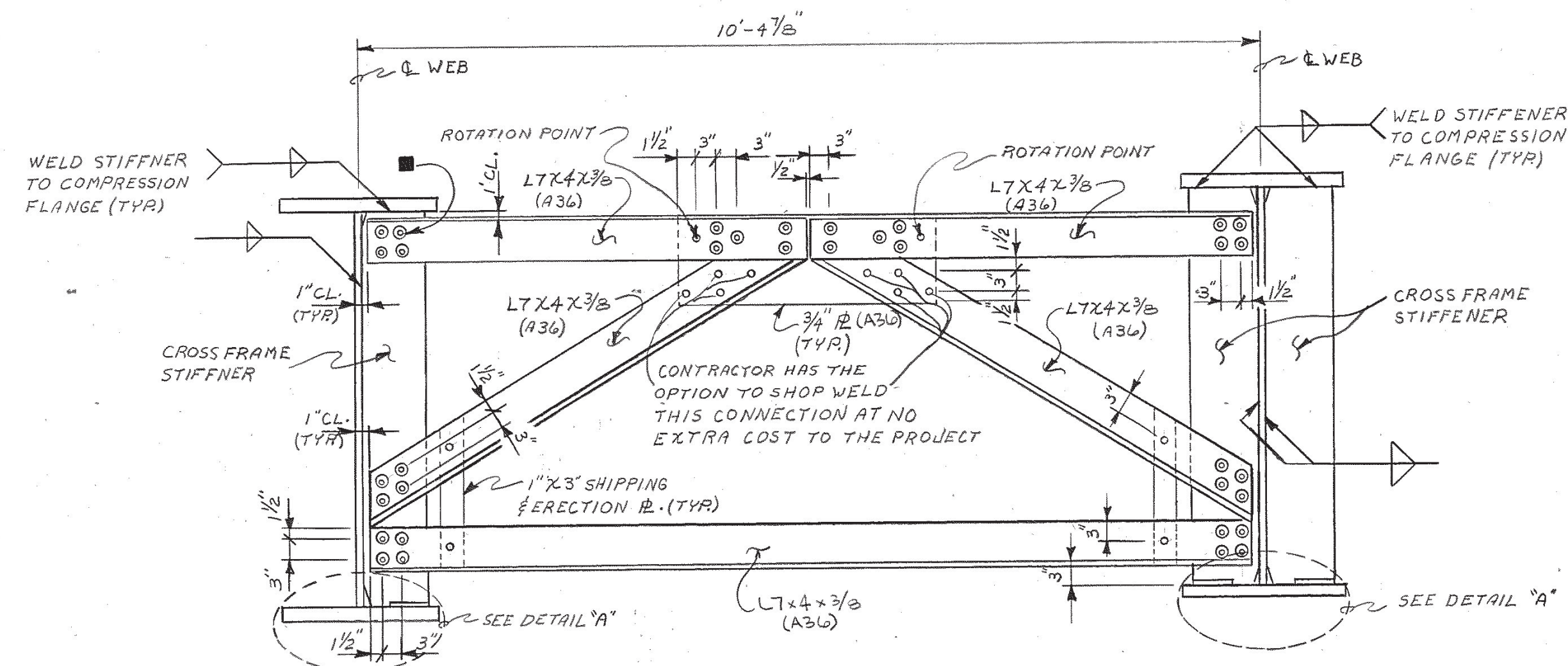
NOTE: CROSS FRAMES BETWEEN BEAMS OF PHASE I AND PHASE II CONSTRUCTION ARE TO BE ERECTED WITH PHASE II CROSS FRAMES WITH BOLTS TO BE HAND TIGHTENED UNTIL PHASE II SLAB IS POURED.

NOTE: RADIUS PLACED IN GIRDER
AT CONTRACTOR'S REQUEST TO
EASE FABRICATION OF GIRDERS.



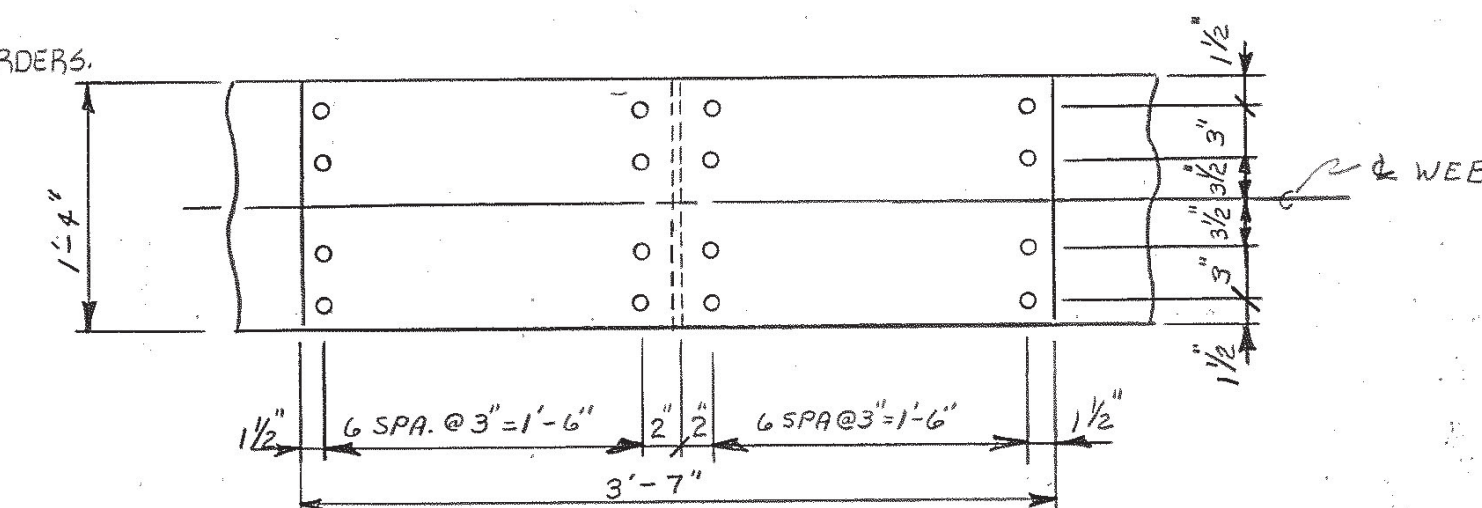
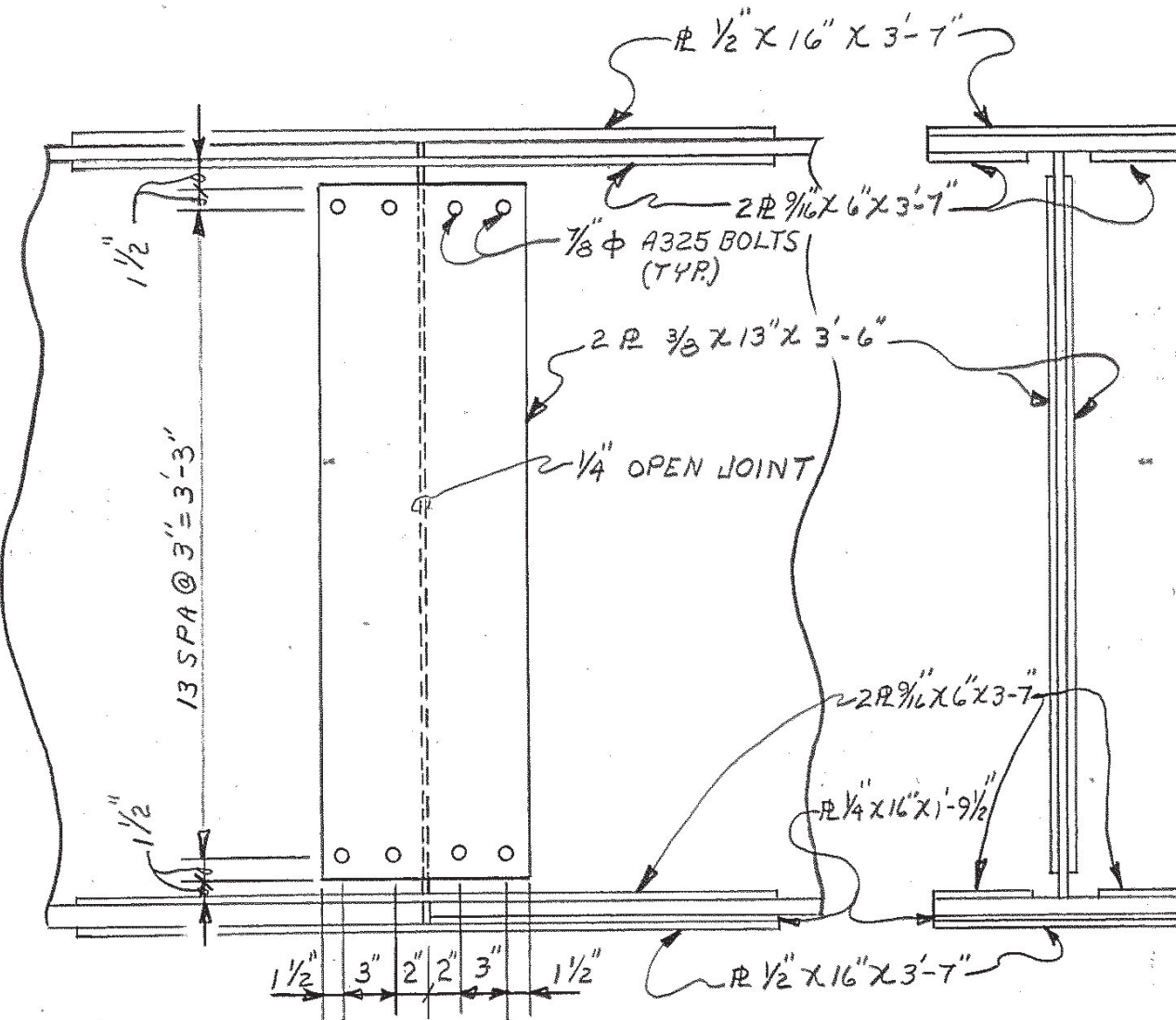
PLAN OF GIRDER @ BENT NO. 1
SHOWING SUPERIMPOSED CURVE
(TYP. ALL GIRDERS)

DETAIL "C"
(SEE DWG. NO. M-246-77)



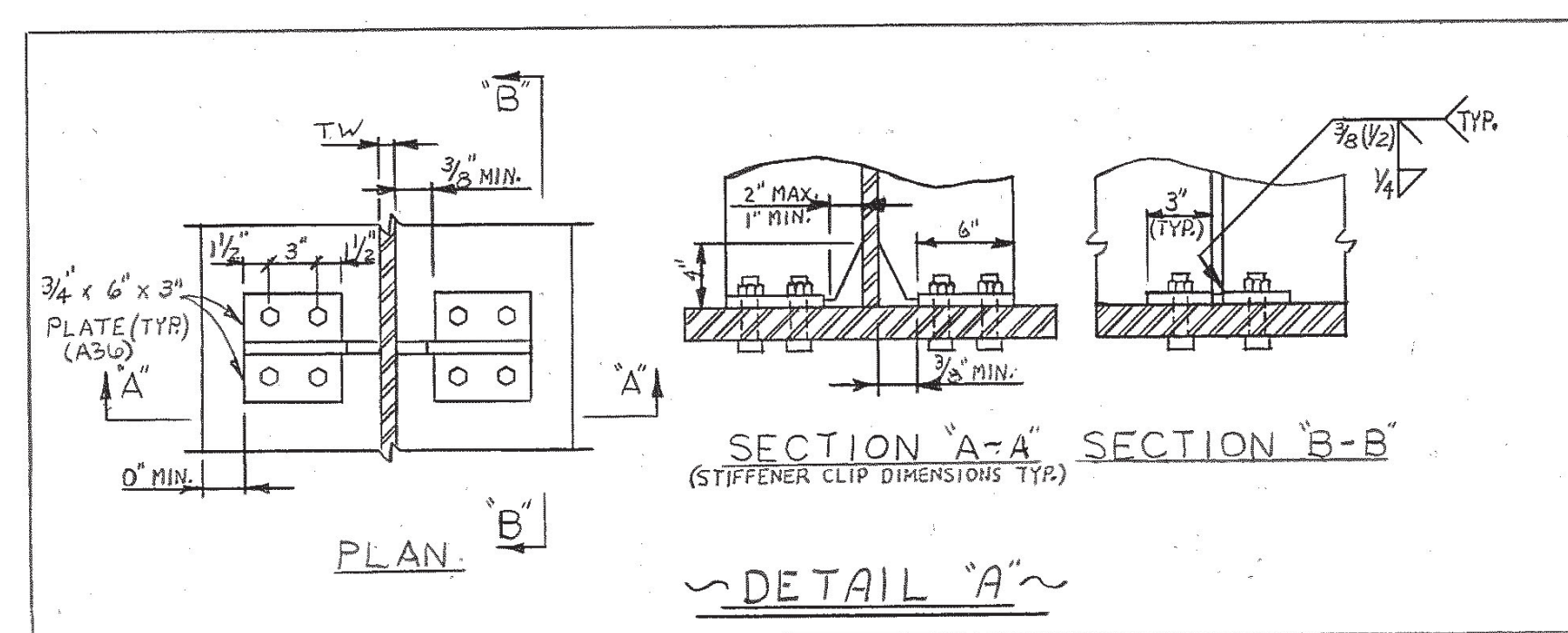
~ TYPICAL CROSS FRAME ~

■ DENOTES: EXTERIOR STIFFENERS AT RIGHT EXTERIOR GIRDER OF PHASE I CONSTRUCTION ARE TO HAVE VERTICALLY SLOTTED HOLES (1/4" X 2") FOR CROSS-FRAME CONNECTION TO ALLOW FOR DIFFERENTIAL DEFLECTION BETWEEN PHASE I AND PHASE II GIRDERS.
(SEE DETAIL BELOW)

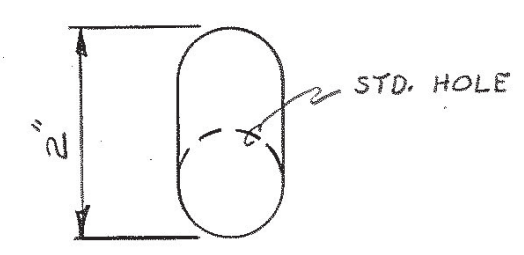


~ FIELD SPLICE ~

NOTE: FOR LOCATION OF TYPE I STIFFNERS AND
TYPE II STIFFNERS SEE FRAMING PLAN DWG. No. M-246-76.

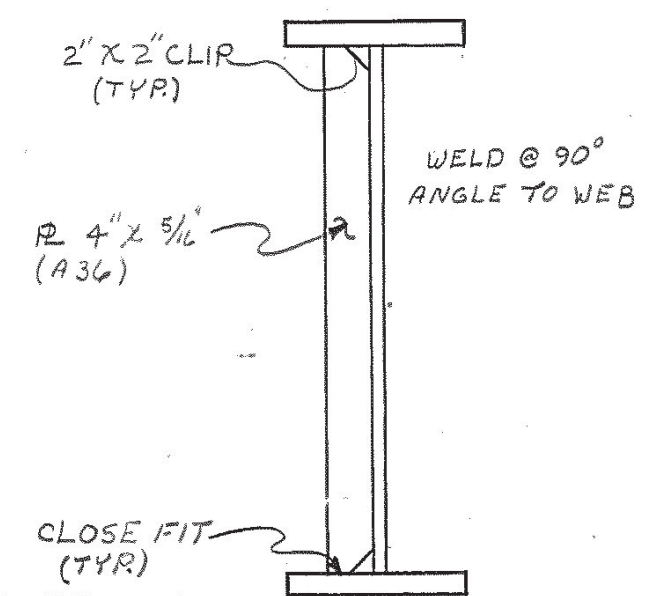


NOTE: TW DENOTES WEB THICKNESS.
NOTE: TYPICAL TOP AND BOTTOM FLANGE.
NOTE: BOLTS TO BE TORQUED TO AASHTO SPECIFICATION FOR:
FRICTION CONNECTION PRIOR TO TAB TO STIFFENER WELD.
NOTE: WEB STIFFENER FIT SHALL BE WITHIN AWS D1-1
FABRICATION TOLERANCE.

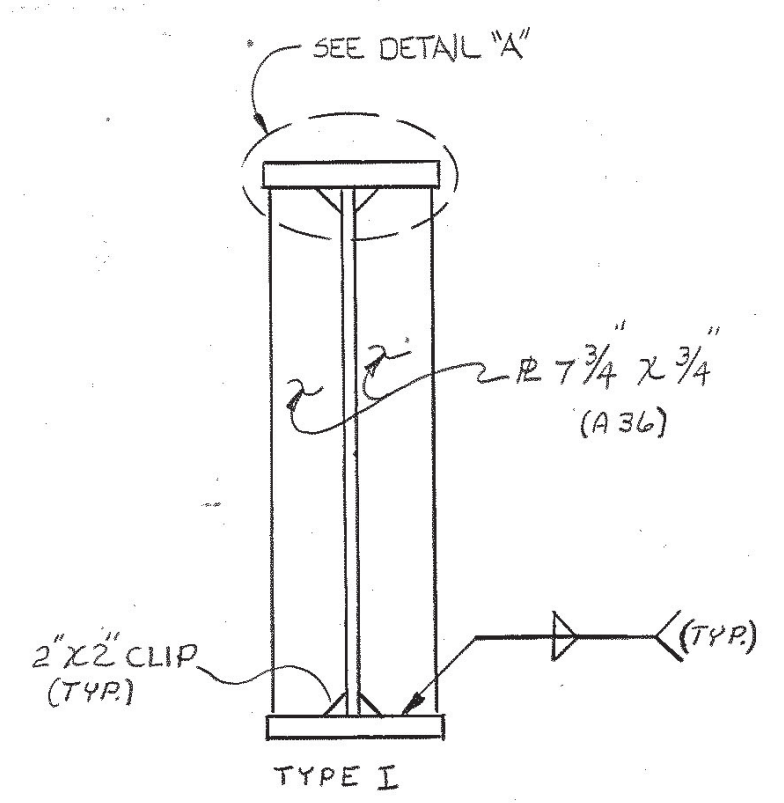


LOCATION OF VERTICAL SLOTTED
HOLE RELATIVE TO STANDARD HOLE

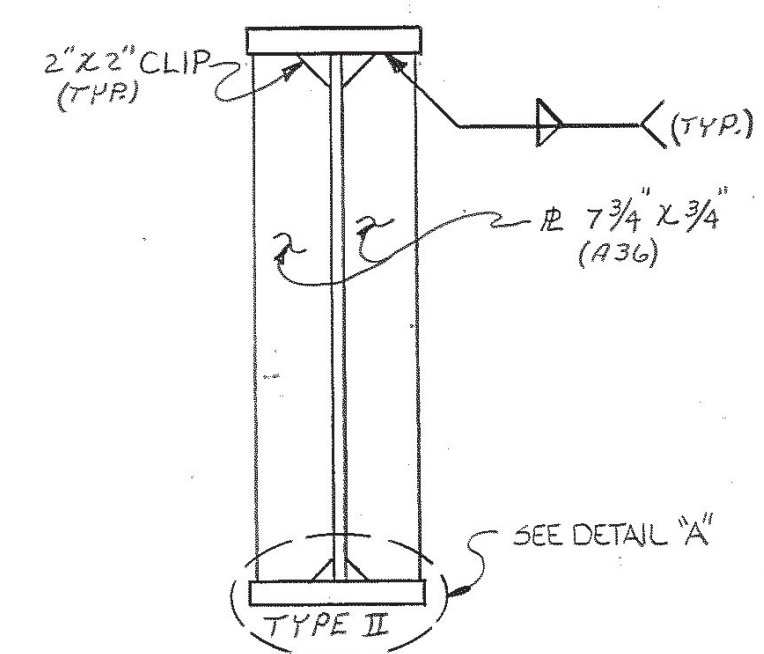
~ SLOTTED HOLE ~



~ INTERMEDIATE STIFFNER ~



~ CROSS-FRAME STIFFNERS ~



NOTE: OMIT STIFFENERS ON EXTERIOR SIDE OF EXTERIOR GIRDERS.

CONST No. 19008-3165-44

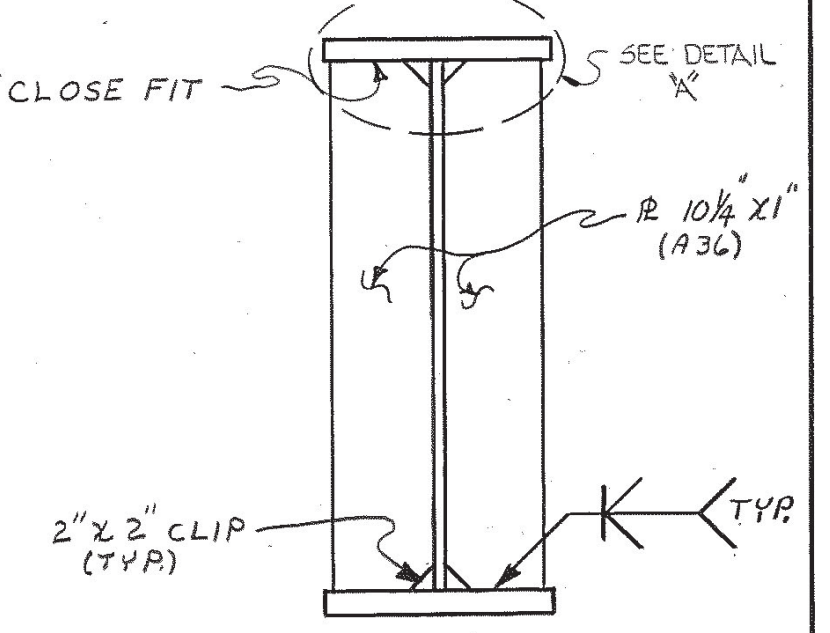
PROJECT NO.	YEAR	SHEET NO.
1R-40-5(106)221	1990	

REVISIONS

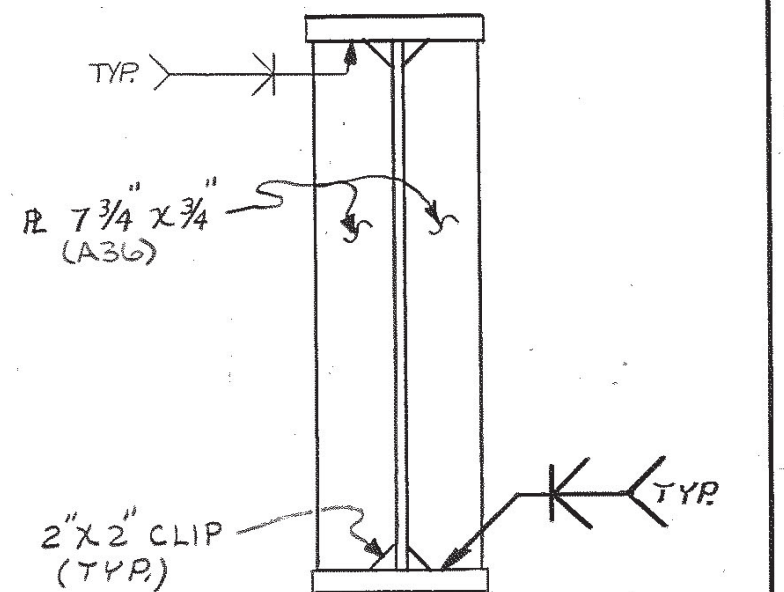
[illegible]

* DENOTES: ON EXTERIOR SIDE
OF EXTERIOR GIRDER ONLY.

*CLOSE FIT SEE DETAIL 'A'



BEARING STIFFNER
~ @ BENT ~



BEARING STIFFNER
~ @ ABUTMENTS ~

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

STRUCTURAL STEEL DETAILS
OLD HICKORY BOULEVARD
OVER INTER-STATE 40 EAST
STATION 125+42.05
LOG MILE 29.31
DAVIDSON COUNTY
1990

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

M-246-77

TELEDYNE POST N85951

DESIGNED BY DAVID HARRELL DATE 7-90
 DRAWN BY JOHN PEASE DATE 9-90
 SUPERVISED BY H. BROOKS / R. HARRISON DATE 7-90/9-90
 CHECKED BY DAVID HARRELL DATE 10/90

CONST. NO. 19008-3165-44

PROJECT NO.		YEAR	SHEET NO.
PR40-5(10)221		1990	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	7-10-91	DTH	REVISED LOCATION OF PHASE I CONST. JT.

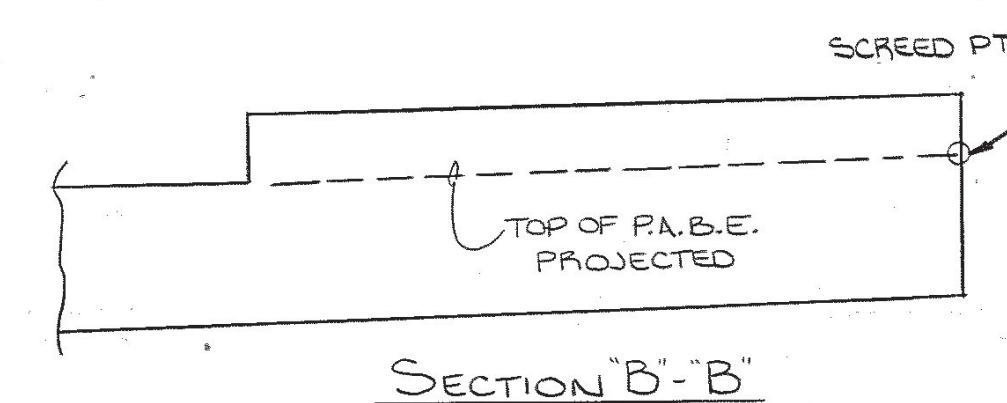
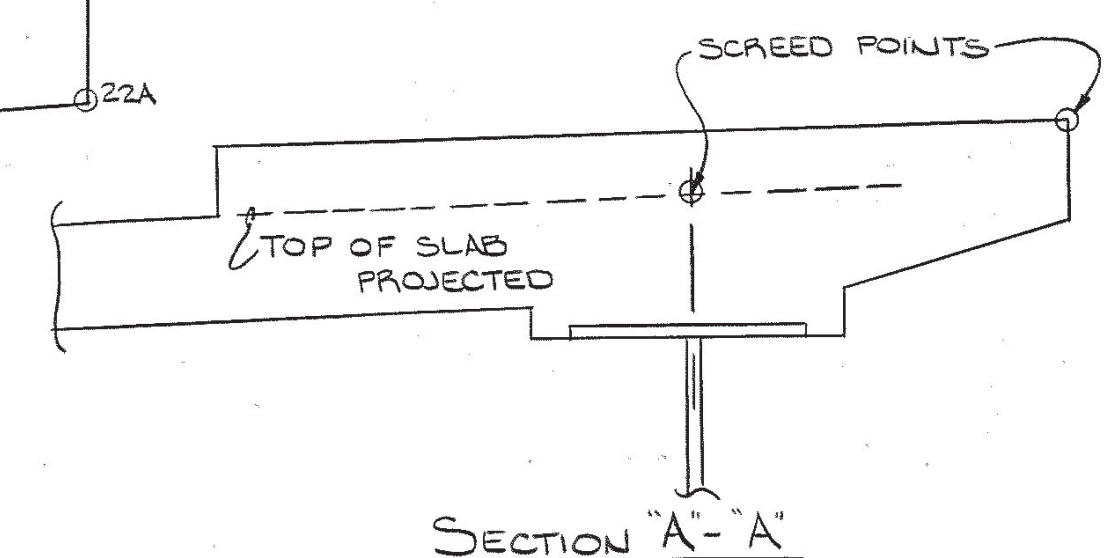
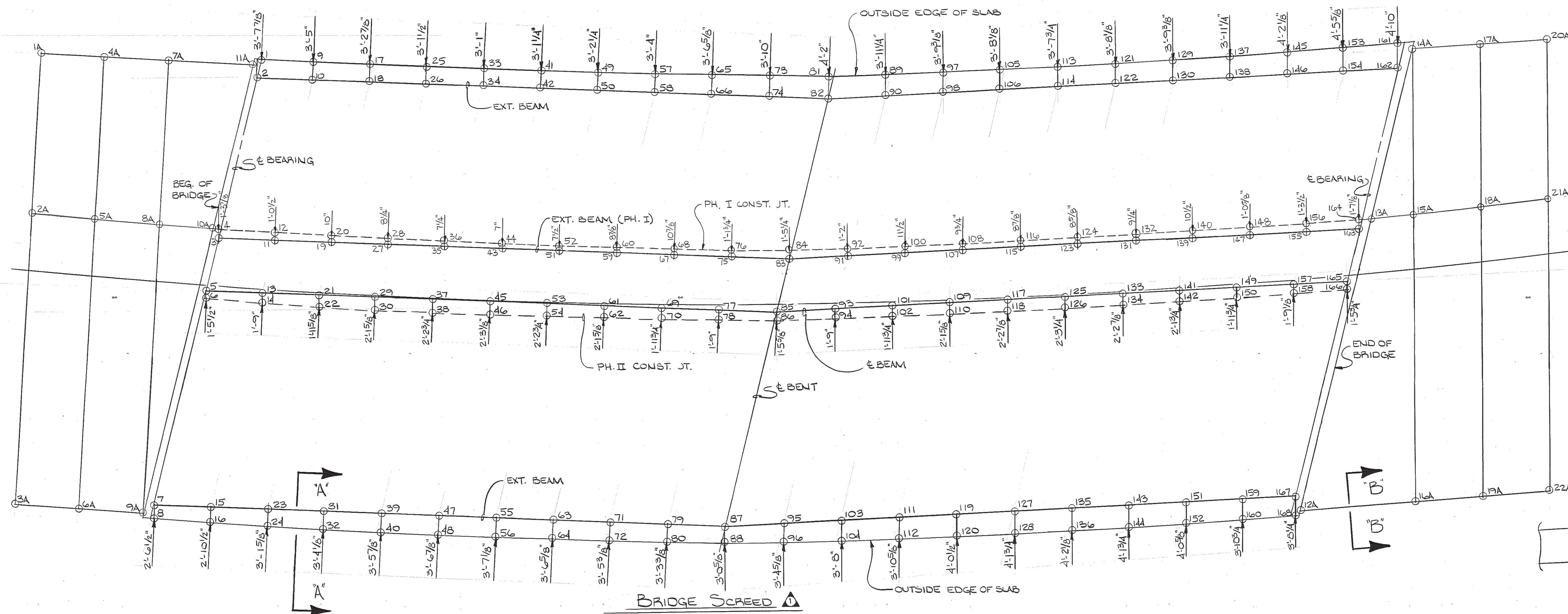


TABLE OF STATIONS, POINTS, & ELEVATIONS

STATION	POINT	ELEV.	STATION	POINT	ELEV.	STATION	POINT	ELEV.	STATION	POINT	ELEV.	STATION	POINT	ELEV.	STATION	POINT	ELEV.	STATION	POINT	ELEV.	STATION	POINT	ELEV.
124+39.76	1	520.13	124+53.92	22	520.92	124+89.14	43	520.58	124+99.80	64	522.85	125+41.91	83	520.92	125+85.86	106	519.37	125+85.95	127	522.09	126+32.89	148	520.05
124+39.76	2	519.42	124+45.95	23	522.21	124+89.14	44	520.56	125+29.63	65	520.26	125+41.91	86	520.97	125+77.60	107	520.43	125+85.95	128	522.65	126+29.91	149	520.46
124+33.85	3	520.44	124+45.95	24	522.74	124+86.92	45	520.92	125+29.68	66	519.55	125+32.11	87	522.31	125+77.60	108	520.40	126+29.91	129	519.87	126+29.91	150	520.53
124+33.85	4	520.39	124+73.48	25	520.26	124+86.92	46	521.00	125+22.32	67	520.59	125+32.11	88	522.84	125+74.90	109	520.79	126+29.91	130	519.17	126+18.27	151	521.90
124+31.93	5	520.78	124+73.48	26	519.52	124+78.26	47	522.29	125+22.32	68	520.56	125+32.11	89	520.18	125+74.90	110	520.86	126+18.27	131	520.25	126+18.27	152	522.46
124+31.93	6	520.83	124+67.02	27	520.54	124+78.26	48	522.83	125+19.92	69	520.94	125+32.11	90	519.48	125+64.41	111	522.20	126+18.27	132	520.22	126+53.27	153	519.59
124+24.43	7	522.13	124+67.02	28	520.51	125+07.20	49	520.27	125+19.92	70	521.01	125+32.11	91	520.53	125+64.41	112	522.75	126+18.27	133	520.64	126+53.27	154	518.91
124+24.43	8	522.64	124+64.92	29	520.83	125+07.20	50	519.55	125+10.57	71	522.32	125+32.11	92	520.49	125+64.41	113	520.02	126+18.27	134	520.69	126+43.95	155	520.01
124+51.00	9	520.17	124+64.92	30	520.95	125+00.20	51	520.59	125+10.57	72	522.86	125+32.11	93	520.88	125+64.41	114	519.31	126+18.27	135	522.04	126+43.95	156	519.96
124+51.00	10	519.46	124+56.72	31	522.24	125+00.20	52	520.56	125+40.92	73	520.24	125+32.11	94	520.94	125+64.41	115	520.33	126+18.27	136	522.60	126+40.90	157	520.37
124+44.91	11	520.48	124+56.72	32	522.78	124+97.92	53	520.93	125+40.92	74	519.54	125+32.11	95	522.28	125+64.41	116	520.35	126+18.27	137	519.79	126+40.90	158	520.43
124+44.91	12	520.44	124+84.72	33	520.26	124+97.92	54	521.01	125+33.38	75	520.58	125+32.11	96	522.82	125+64.41	117	520.73	126+18.27	138	519.09	126+29.04	159	521.83
124+42.92	13	520.82	124+84.72	34	519.53	124+89.03	55	522.30	125+33.38	76	520.54	125+32.11	97	520.13	125+64.41	118	520.81	126+18.27	139	520.17	126+29.04	160	522.39
124+42.92	14	520.88	124+78.03	35	522.84	124+89.03	56	522.84	125+30.92	77	520.93	125+32.11	98	519.43	125+64.41	119	522.15	126+18.27	140	520.14	126+64.50	161	519.48
124+35.19	15	522.17	124+78.03	36	520.54	125+18.44	57	520.27	125+30.92	78	520.99	125+32.11	99	520.49	125+64.41	120	522.71	126+18.27	141	520.53	126+64.50	162	518.81
124+35.19	16	522.69	124+75.92	37	520.90	125+18.44	58	519.55	125+21.34	79	522.32	125+32.11	100	520.45	126+08.33	121	519.94	126+18.27	142	520.60	126+55.00	163	519.92
124+62.24	17	520.21	124+75.92	38	520.98	125+11.26	59	520.59	125+21.34	80	522.85	125+32.11	101	520.84	126+08.33	122	519.24	126+18.27	143	521.97	126+55.00	164	519.86
124+62.24	18	519.49	124+67.49	39	522.27	125+11.26	60	520.56	125+52.15	81	520.21	125+32.11	102	520.91	126+08.33	123	520.31	126+18.27	144	522.53	126+51.90	165	520.29
124+55.96	19	520.51	124+67.49	40	522.81	125+08.92	61	520.94	125+52.15	82	519.52	125+32.11	103	522.24	126+08.33	124	520.28	126+18.27	145	519.69	126+51.90	166	520.34
124+55.96	20	520.48	124+95.96	41	520.27	125+08.92	62	521.01	125+44.43	83	520.57	125+32.11	104	522.79	126+08.33	125	520.67	126+18.27	146	519.00	126+29.81	167	521.75
124+53.92	21	520.85	124+95.96	42	519.55	124+99.80	63	522.31	125+44.43	84	520.52	125+32.11	105	520.07	126+08.33	126	520.75	126+18.27	147	520.09	126+29.81	168	522.30

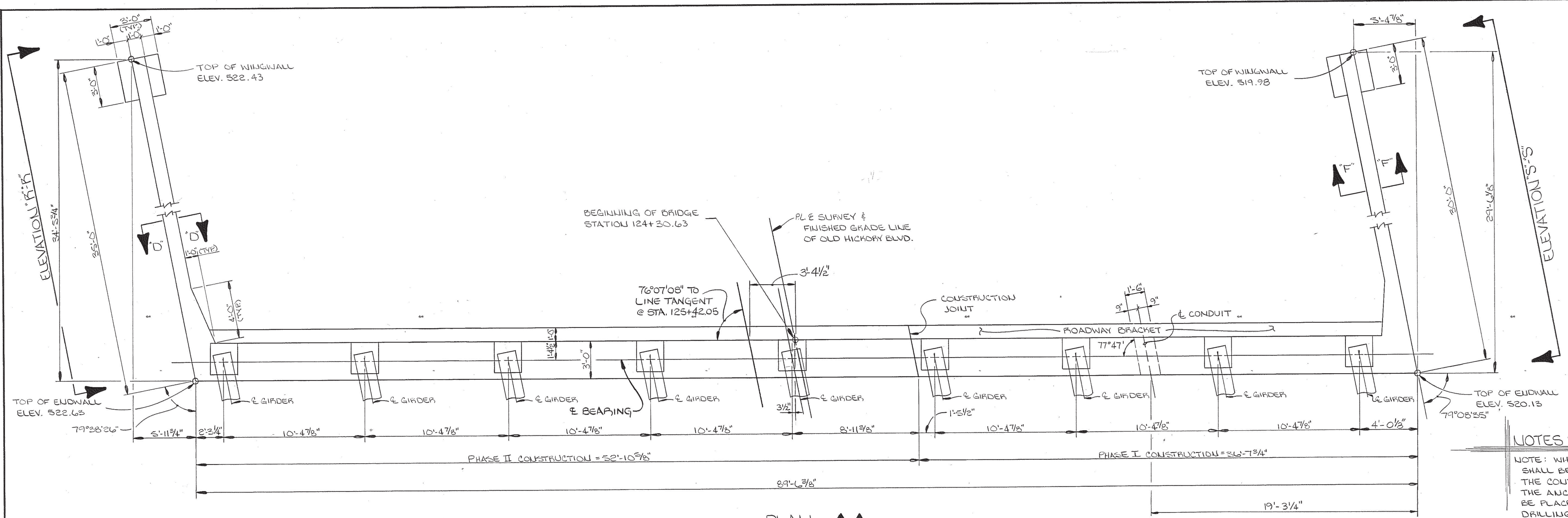
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
BRIDGE SCREED
OLD HICKORY BOULEVARD OVER
INTERSTATE 40 EAST
STATION 125+42.00
LOG MILE 29.31
DAVIDSON COUNTY
1990

DESIGNED BY DAVID HARRELL
DRAWN BY D.W. COLE
SUPERVISED BY ALH - HMB
CHECKED BY DAVID HARRELL
DATE 8/90
DATE 8/90
DATE 8/90
DATE 10/90

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

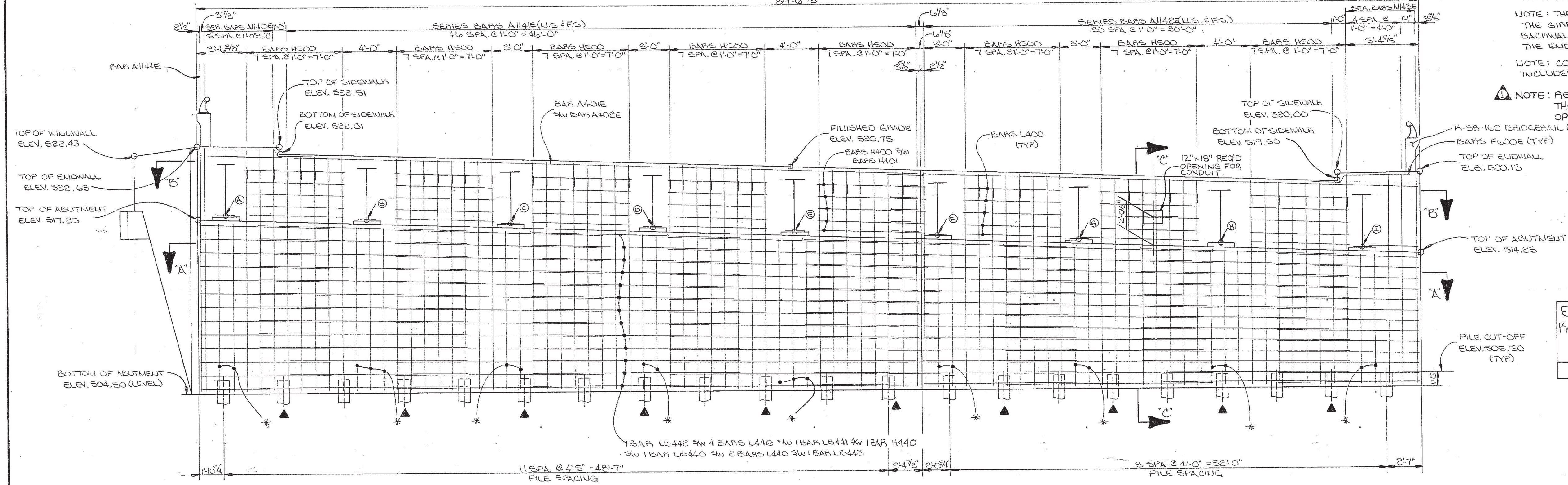
M-246-78

Const. No. 19008-3165-44			
PROJECT NO.		YEAR	SHEET NO.
IR-40-S(106)221		1990	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	1-28-91	OTH	ADDED OPENING FOR TELEPHONE CONDUIT.
2	4-25-91	DTH	CHANGED LOCATION OF TELEPHONE CONDUIT.
</			



PLAN 1 2
87'-6 3/8"

- NOTES:**
- NOTE: WHEN POURING ABUTMENT BEAM, PROVISIONS SHALL BE MADE FOR SETTING ANCHOR BOLTS. IF THE CONTRACTOR ELECTS TO DRILL THE HOLES FOR THE ANCHOR BOLTS, THE REINFORCING STEEL SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE DRILLING. BOLT PROJECTION = 4"
 - NOTE: RISER BLOCKS TO BE POURED MONOLITHICALLY WITH ABUTMENT BEAM.
 - NOTE: THE BACKWALL SHALL NOT BE POURED UNTIL THE GIRDERS ARE IN PLACE. THE TOP 12" OF THE BACKWALL SHALL BE POURED CONCURRENTLY WITH THE END OF SLAB.
 - NOTE: COST OF BRIDGE RAIL AND POST IS TO BE INCLUDED IN THE COST OF BRIDGE RAIL SYSTEM.
 - NOTE: REINFORCEMENT TO BE CUT OR MOVED IN THE FIELD TO ACCOMMODATE CONDUIT OPENING.



ELEVATION 1 2
(LOOKING BACK ON SURVEY)

ESTIMATED QUANTITIES

EPOXY BAR REINFORCEMENT	CLASS "A" CONCRETE	STEEL BAR REINFORCEMENT
LBS.	CY.	LBS.
16,080	137	7,604

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS

ABUTMENT NO. 1
 OLD HICKORY BOULEVARD
 OVER INTERSTATE 40 EAST
 STATION 125+42.05 LOG MILE 29.31
 DAVIDSON COUNTY
 1990

DESIGNED BY DAVID HARRELL
 DRAWN BY DONNA DOUGLAS
 SUPERVISED BY B. L. HARRISON
 CHECKED BY D. HARRELL

DATE 7-90
 DATE 8-90
 DATE 8-90
 DATE 10-90

RISER BLOCK POINTS AND ELEVATIONS

A	B	C	D	E	F	G	H	I
517.20	516.96	516.62	516.28	515.95	515.61	515.27	514.93	514.59

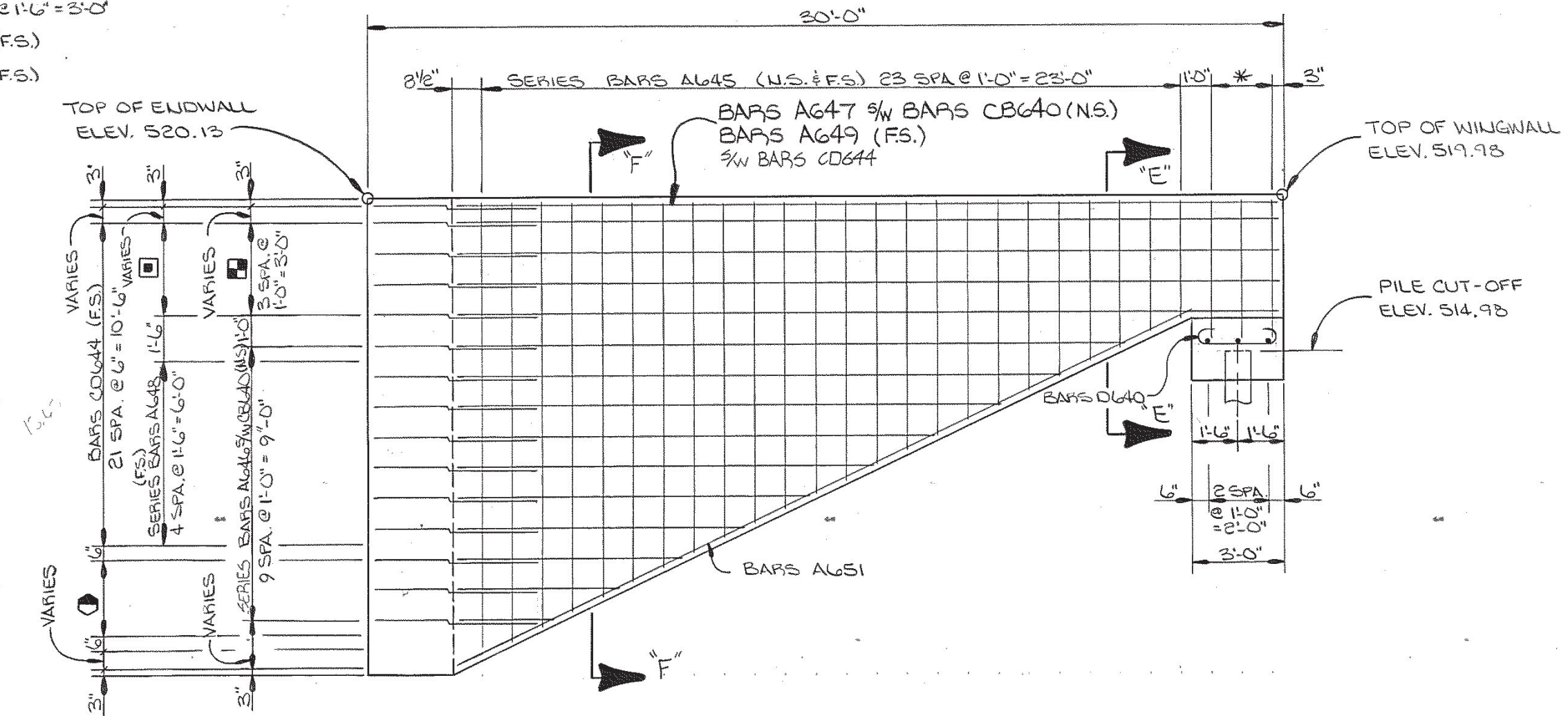
* DENOTES BARS TO BE CUT IN FIELD.
 (U.S.) DENOTES NEAR SIDE.
 (F.S.) DENOTES FAR SIDE.

▲ DENOTES BATTER PILE @ 2:12

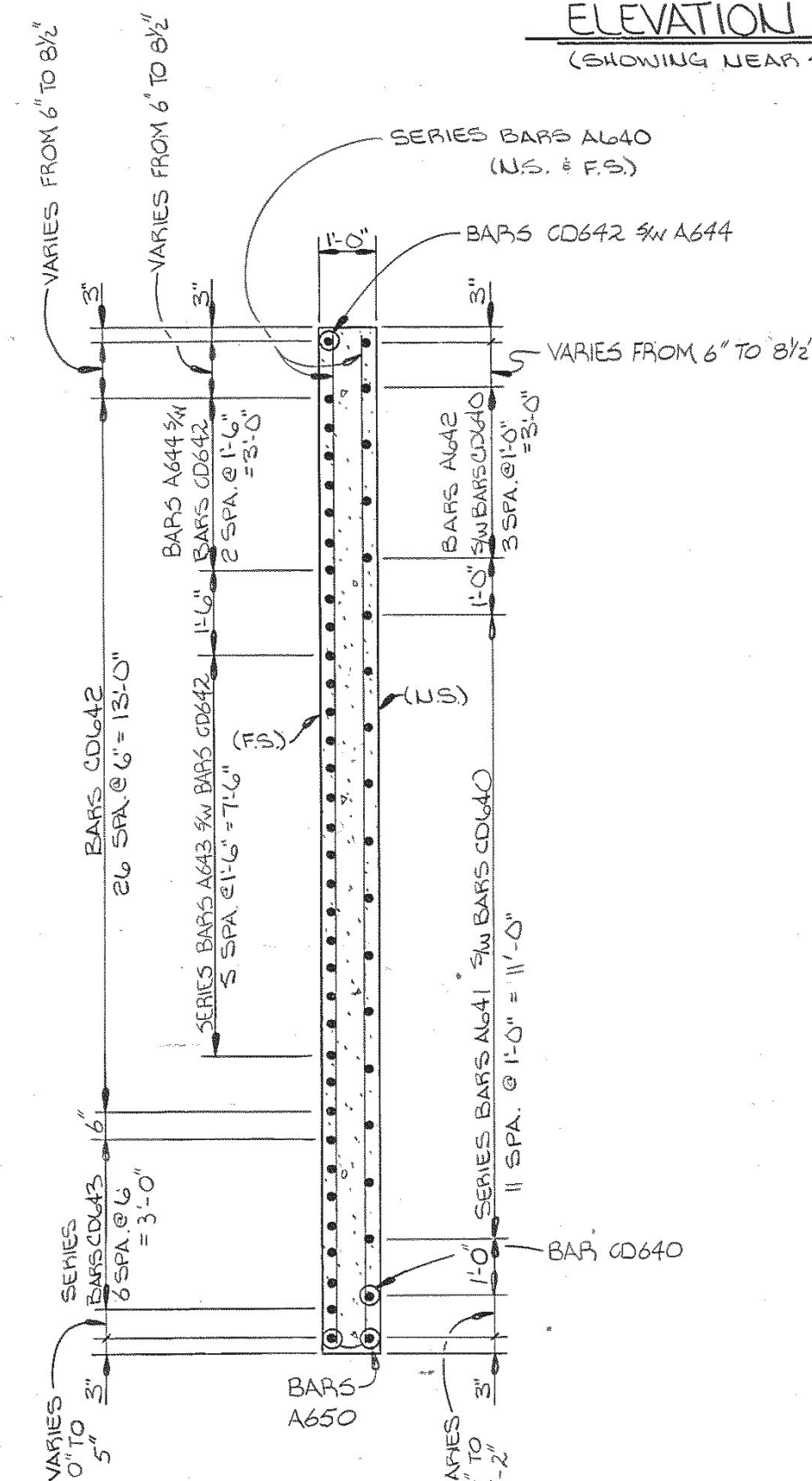
CORRECT *Edward P. Wasserman*
 ENGINEER OF STRUCTURES

APPROVED _____
 DIRECTOR OF HIGHWAYS

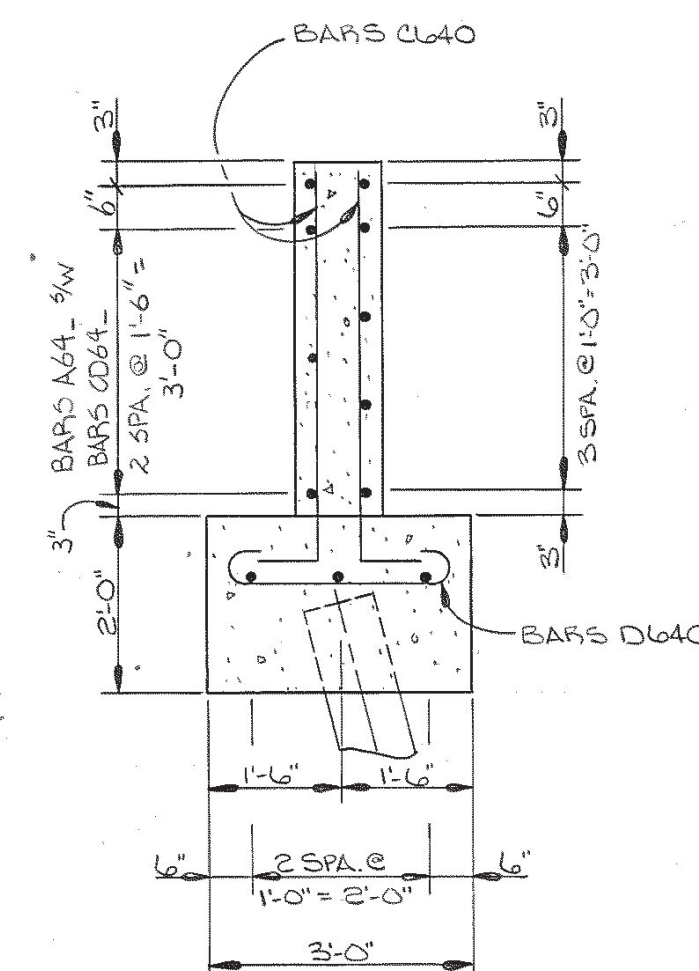
- * DENOTES BARS CL60 2 SFA. @ 1'-0" x 2'-0"
- DENOTES BARS AL642 4W BARS CL6040 (U.S.)
- DENOTES BARS AL644 4W BARS OD642 (F.S.) 2 SFA. @ 1'-6" = 3'-0"
- DENOTES BARS AL647 4W BARS CL6040 (U.S.)
- DENOTES BARS AL648 (F.S.) 2 SFA. @ 1'-6" = 3'-0"
- DENOTES SERIES BARS CL643 (F.S.)
5 SFA. @ 1'-0" = 3'-0"
- DENOTES SERIES BARS CL645 (F.S.)
6 SFA. @ 1'-0" = 3'-0"

[illegible]

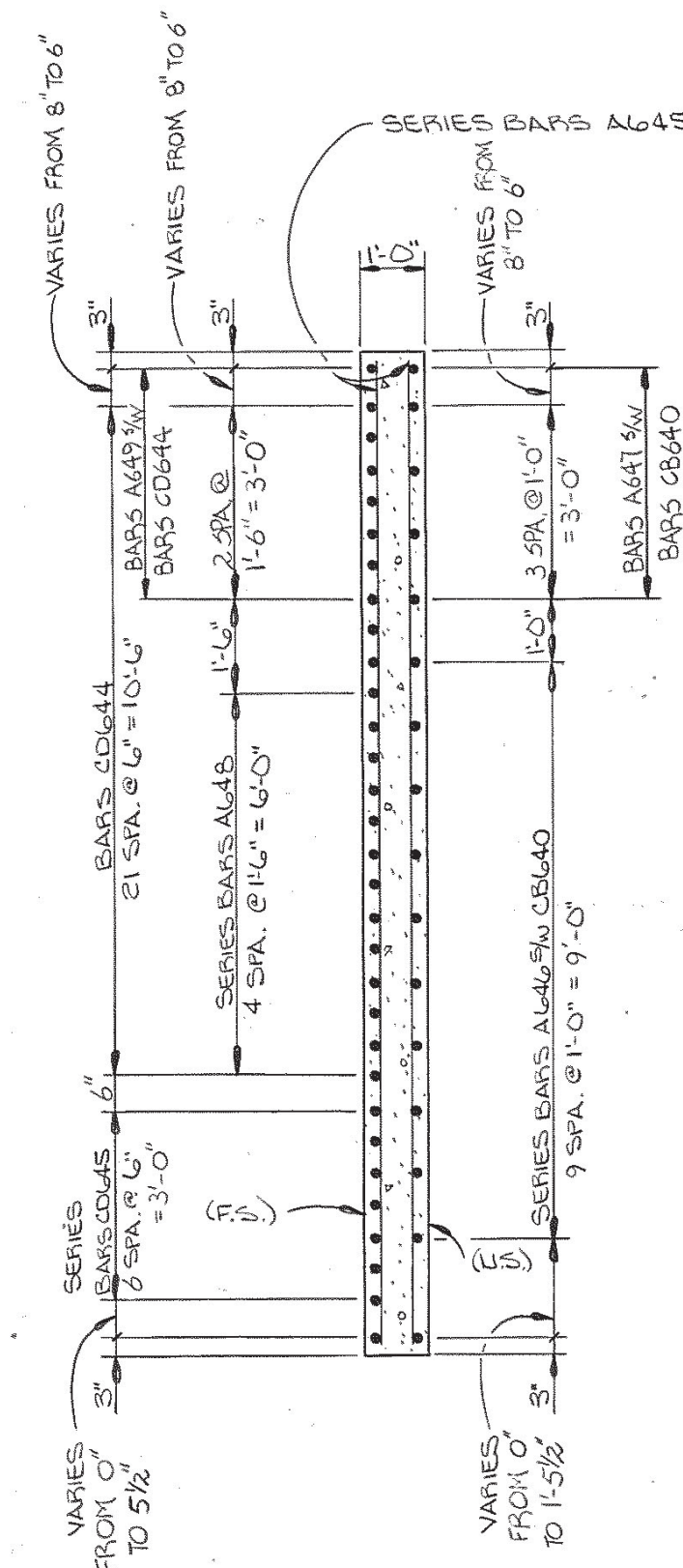
ELEVATION "R"-"R"
(SHOWING NEAR SIDE STEEL ONLY)



SECTION "D"- "D"

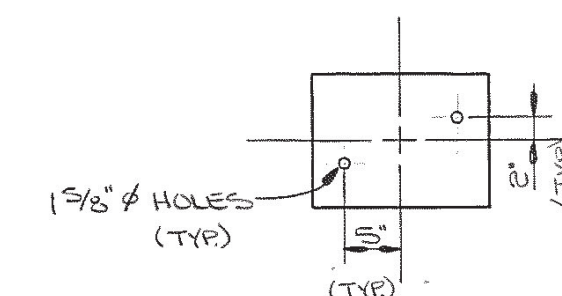


SECTION "E" - "E"

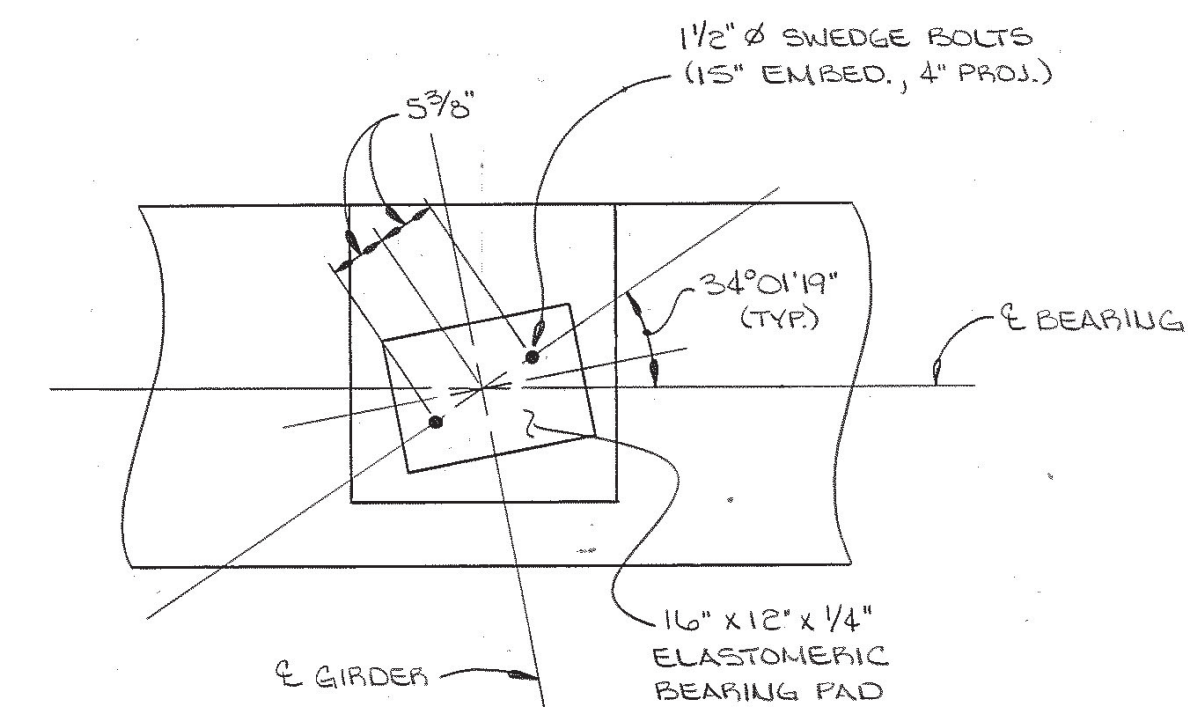


SECTION "F"-"F"

ELEVATION "S"-S"
(SHOWING NEAR SIDE STEEL ONLY)



ELASTOMERIC BEARING PAD



ELASTOMERIC BEARING PAD DETAILS

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

ABUTMENT NO. 1 DETAILS
OLD HICKORY BOULEVARD
OVER INTERSTATE 40 EAST
STATION 125+42.05 LOG MILE 29.31
DAVIDSON COUNTY
1990

DESIGNED BY DAVID HARRELL DATE 7-90
 DRAWN BY DONNA DOUGLAS DATE 8-90
 SUPERVISED BY R.L. HARRISON DATE 8-90
 CHECKED BY DAVID HARRELL DATE 10-90

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

APPROVED _____
DIRECTOR OF HIGHWAYS

1-246-81

CONST. No. 19008-3165-44

PROJECT NO.	YEAR	SHEET NO.
1R-40-5(10)221	1990	

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION
1	1-28-91	DTH	ADDED OPENING FOR TELEPHONE CONDUIT.
2	4-25-91	DTH	CHANGED LOCATION OF TELEPHONE CONDUIT

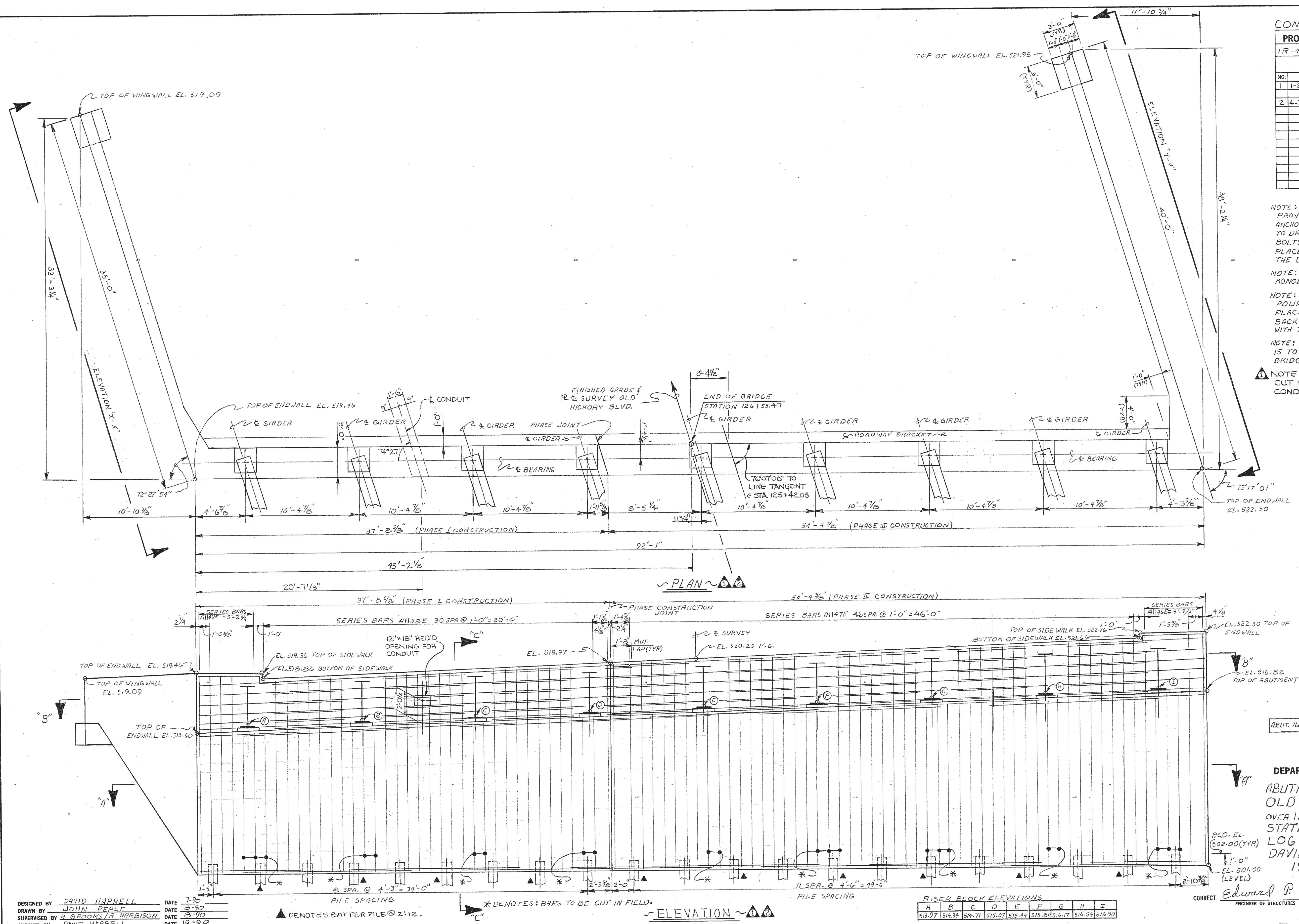
NOTE: WHEN POURING ABUTMENT BEAM, PROVISIONS SHALL BE MADE FOR SETTING ANCHOR BOLTS. IF THE CONTRACTOR ELECTS TO DRILL THE HOLES FOR THE ANCHOR BOLTS, THE REINFORCING STEEL SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE DRILLING. BOLT PROJECTION = $\frac{A}{4}$.

NOTE: RISER BLOCKS TO BE POURED MONOLITHICALLY WITH ABUTMENT BEAM.

NOTE: THE BACKWALL SHALL NOT BE POURED UNTIL THE GIRDERS ARE IN PLACE. THE TOP 12" OF THE BACKWALL SHALL BE POURED CONCURRENTLY WITH THE END OF SLAB.

NOTE: COST OF BRIDGE RAIL AND POST IS TO BE INCLUDED IN THE COST OF BRIDGE RAIL SYSTEM.

NOTE: REINFORCEMENT TO BE MOVED OR CUT IN THE FIELD TO ACCOMMODATE CONDUIT OPENING.



CLASS "A"	STEEL BAR	EPOXY
CONCRETE	REINFORCEMENT	STEEL BAR
C.Y.	REINFORCEMENT	REINFORCEMENT
ABUT. No. 2	190	18,461

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
ABUTMENT No. 2
OLD HICKORY BOULEVARD
OVER INTERSTATE 40 EAST
STATION 125+42.05
LOG MILE 29.31
DAVIDSON COUNTY
1990

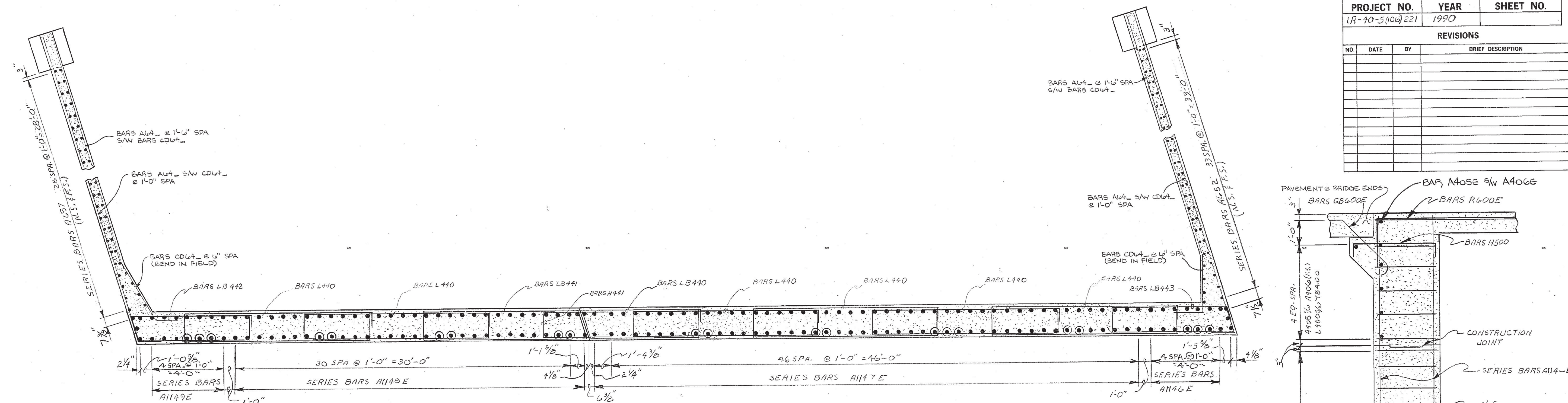
Edward P. Wasserman
ENGINEER OF STRUCTURES

DESIGNED BY DAVID HARRELL DATE 7-95
DRAWN BY JOHN PEARCE DATE 8-90
SUPERVISED BY H. BROOKS JR. HARBISON DATE 8-90
CHECKED BY DAVID HARRELL DATE 10-90

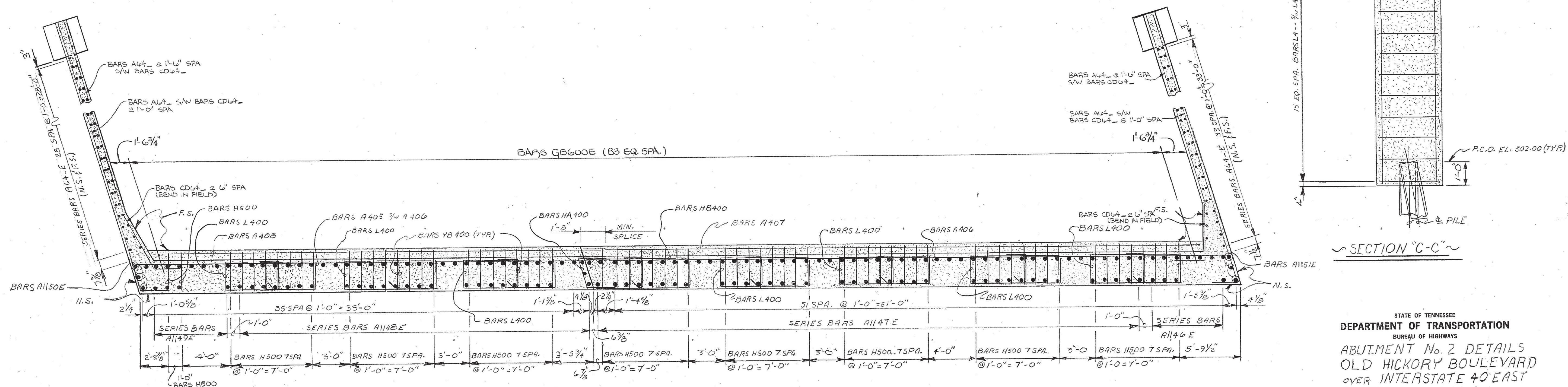
RISER BLOCK ELEVATIONS							
A	B	C	D	E	F	G	H
513.97	514.34	514.71	515.07	515.44	515.81	516.17	516.54

M-246-82

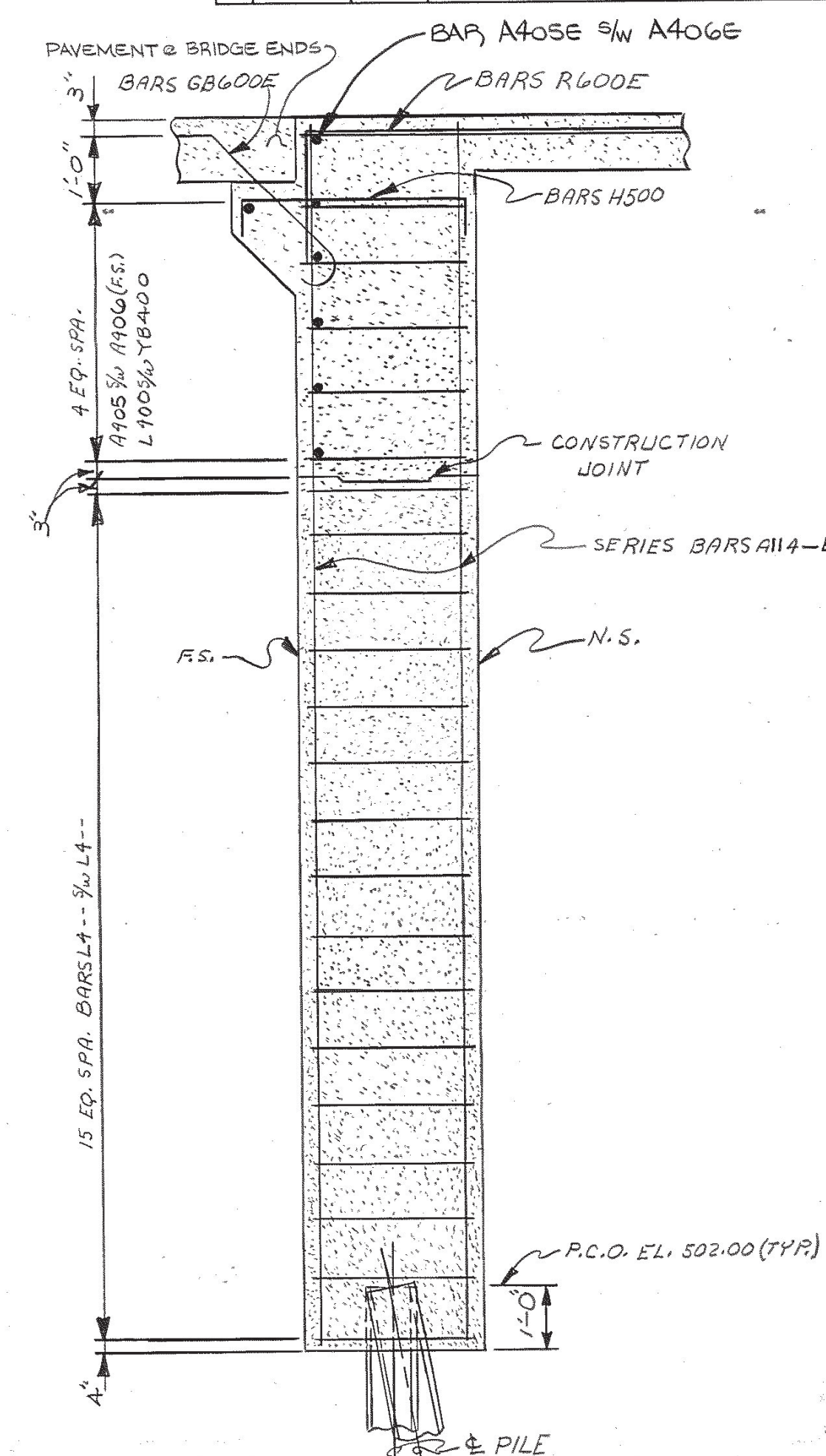
PROJECT NO.	YEAR	SHEET NO.
IR-40-5(106) 221	1990	

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~ SECTION "A-A" ~



~ SECTION 'B-B' ~



~ SECTION "C-C" ~

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

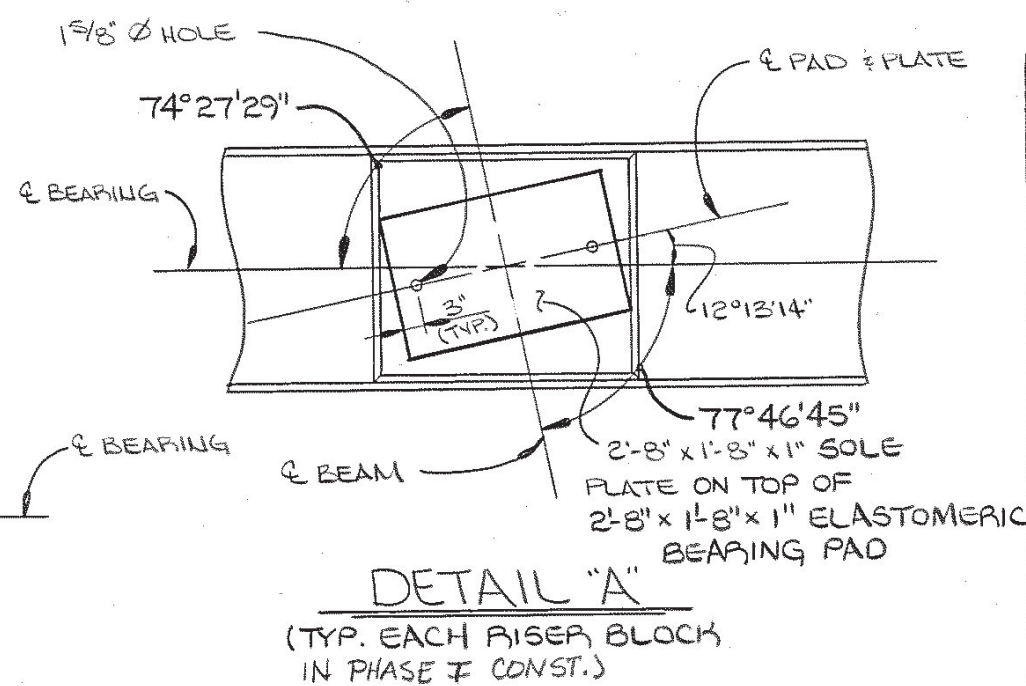
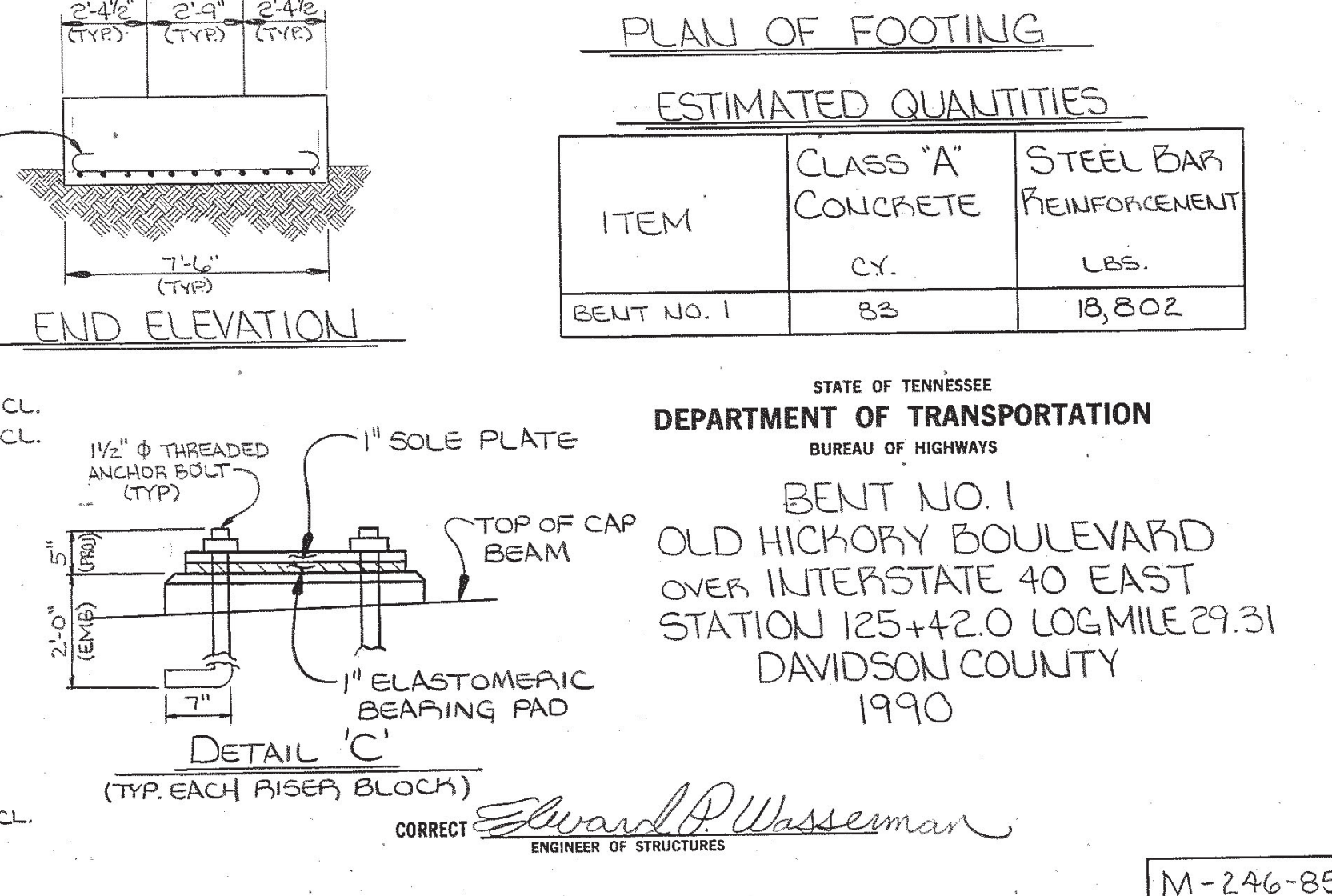
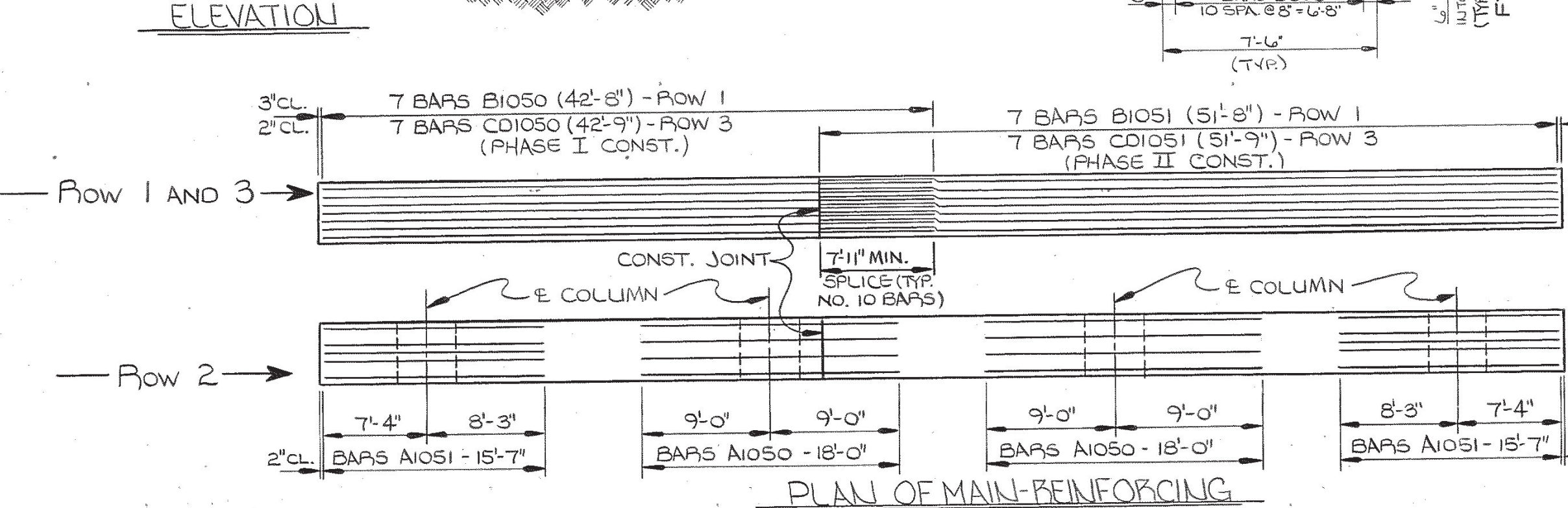
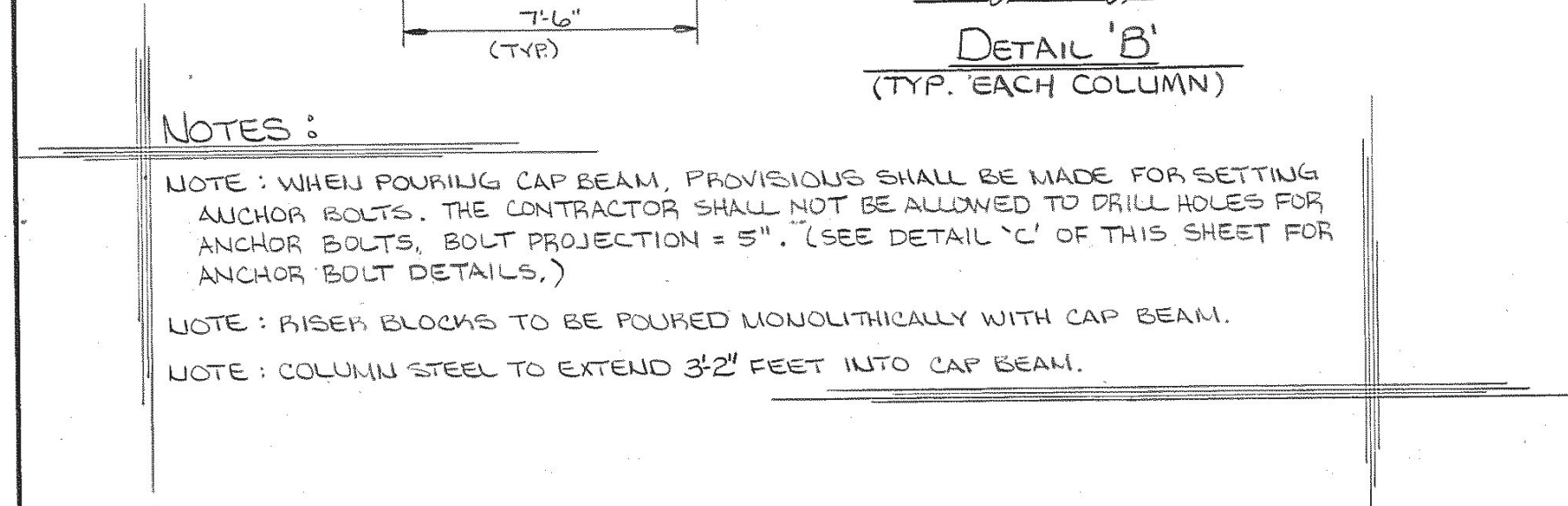
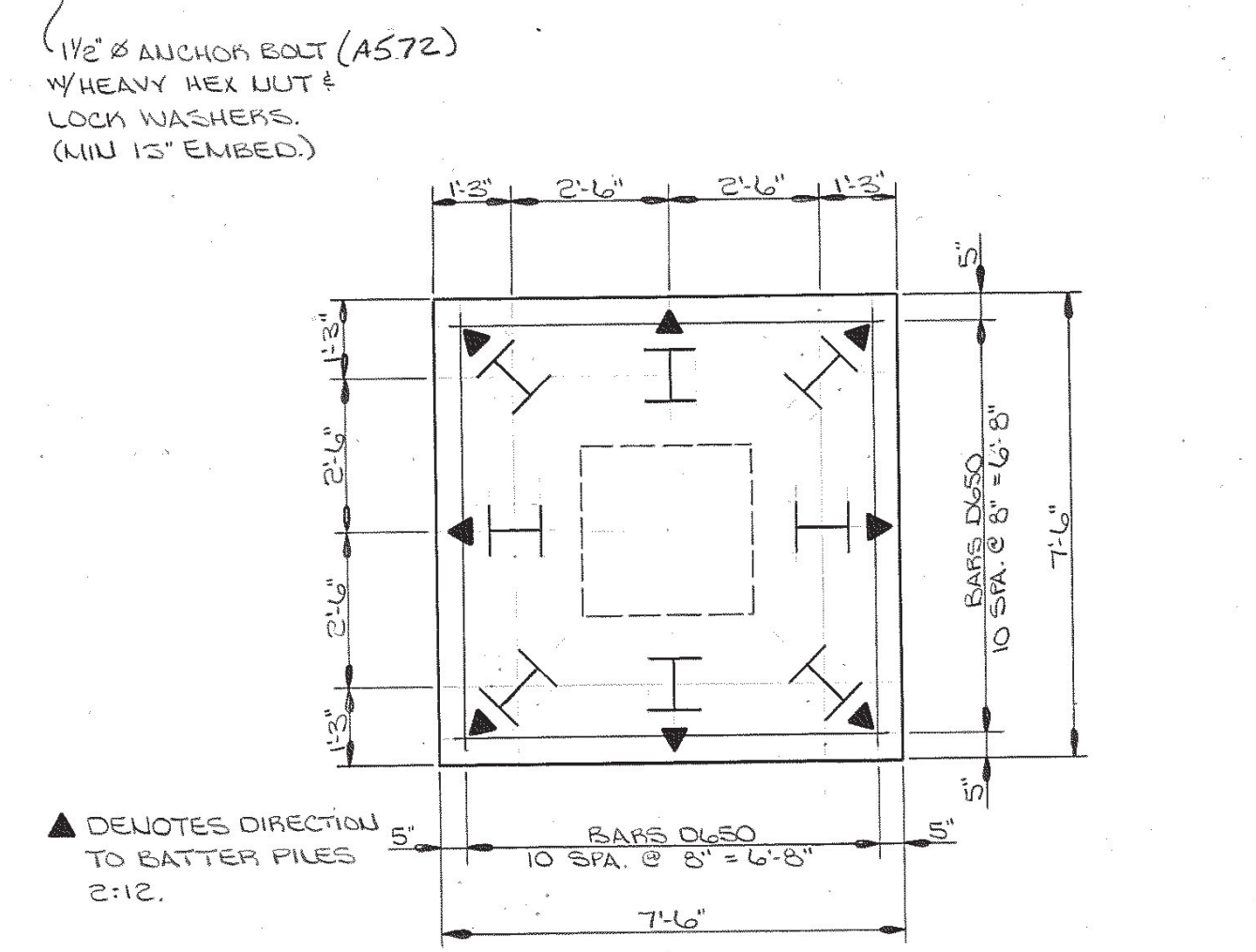
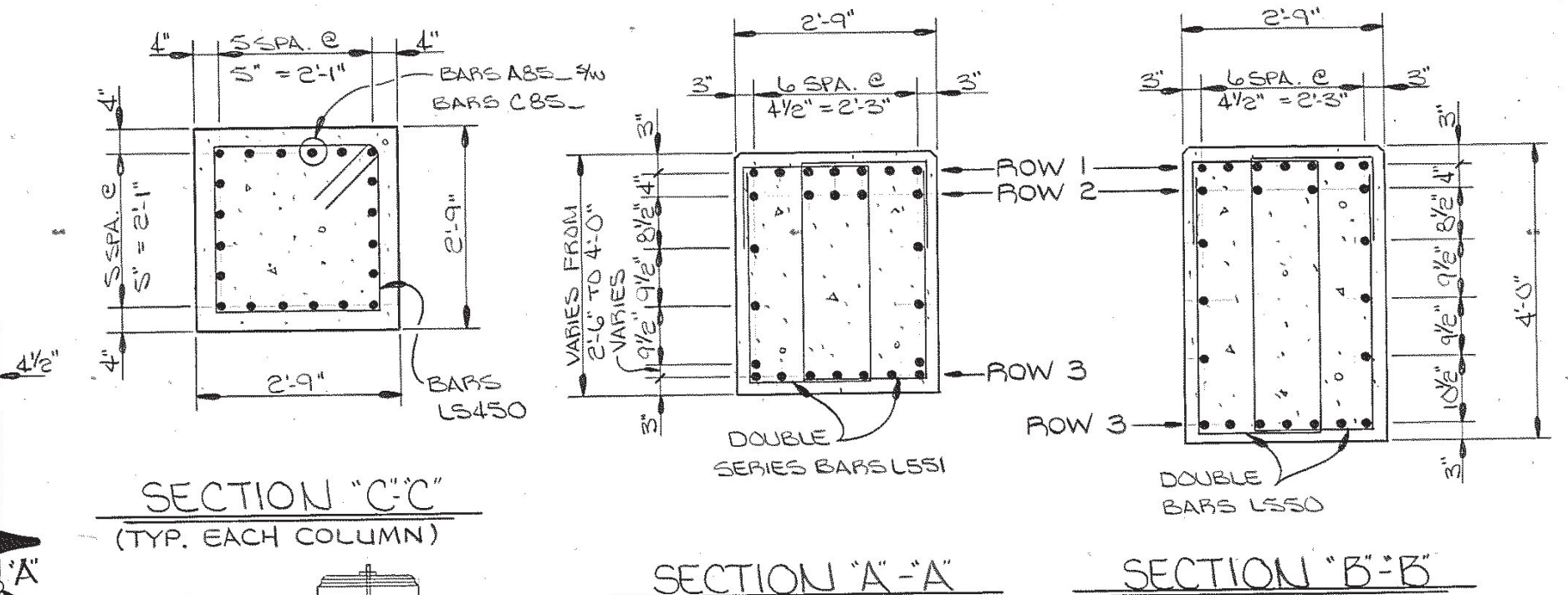
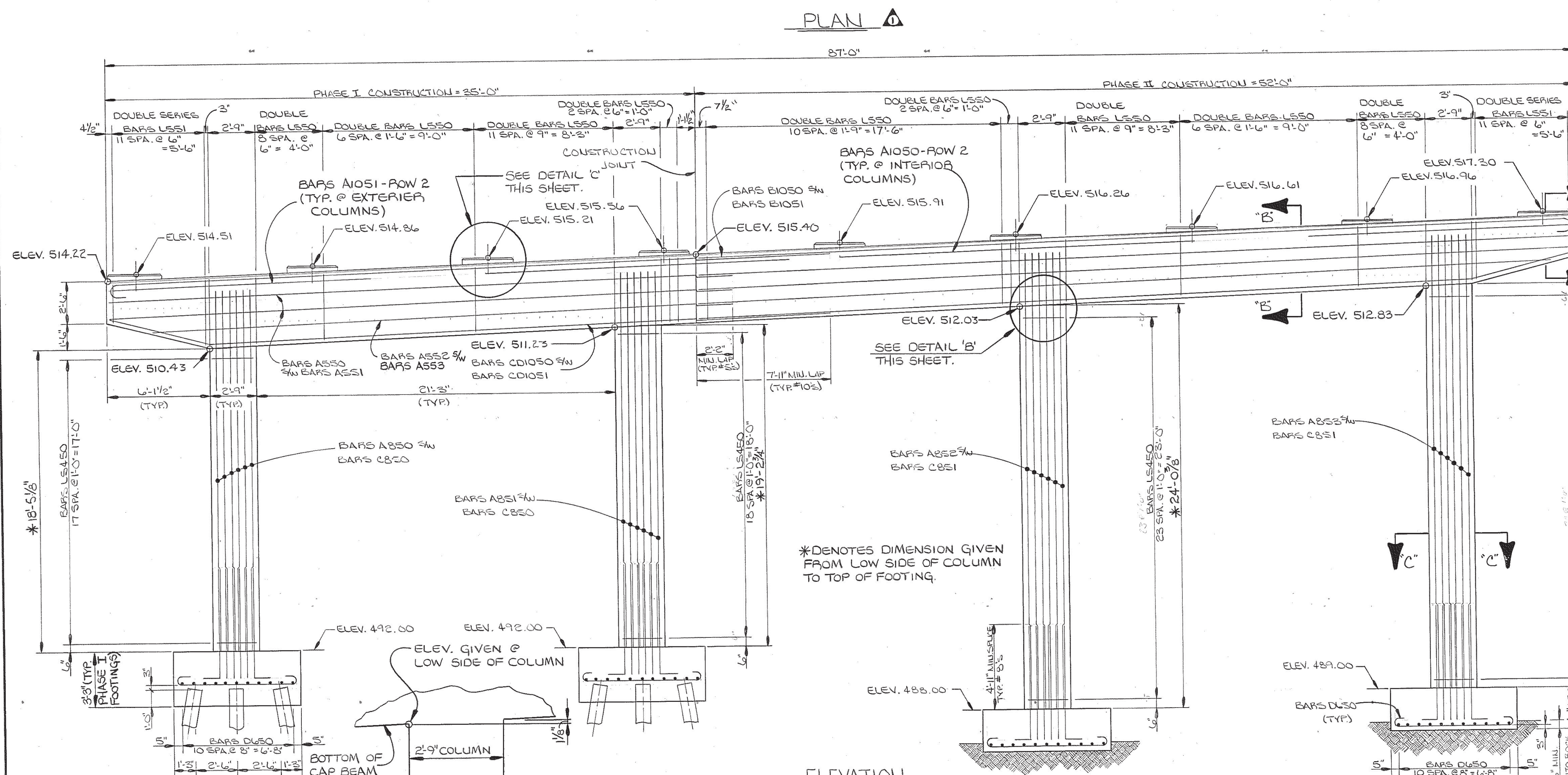
BUREAU OF HIGHWAYS

ABUTMENT No. 2 DETAILS
OLD HICKORY BOULEVARD
OVER INTERSTATE 40 EAST
STATION 125 + 42.05
LOG MILE 29.31
DAVIDSON COUNTY
1990

DESIGNED BY	DAVID HARRELL	DATE	7-90
DRAWN BY	JOHN PEASE	DATE	8-90
SUPERVISED BY	H. BROOKS JR. HARBISON	DATE	8-90
CHECKED BY	DAVID HARRELL	DATE	10-90

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

M-246-83

[illegible]

NOTES :

NOTE : WHEN POURING CAP BEAM, PROVISIONS SHALL BE MADE FOR SETTING ANCHOR BOLTS. THE CONTRACTOR SHALL NOT BE ALLOWED TO DRILL HOLES FOR ANCHOR BOLTS. BOLT PROJECTION = 5". (SEE DETAIL 'C' OF THIS SHEET FOR ANCHOR BOLT DETAILS.)

NOTE : RISER BLOCKS TO BE POURED MONOLITHICALLY WITH CAP BEAM.

NOTE : COLUMN STEEL TO EXTEND 32" FEET INTO CAP BEAM.

DESIGNED BY DAVID HARRELL DATE 6-90
DRAWN BY DAVIDA DOUGLAS & M. DYE DATE 8-90
SUPERVISED BY R. L. HARRISON DATE 8-90
CHECKED BY DAVID HARRELL DATE 10-90

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

BENT NO. 1
OLD HICKORY BOULEVARD
OVER INTERSTATE 40 EAST
STATION 125+42.0 LOG MILE 29.31
DAVIDSON COUNTY
1990

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

M-246-85

BILL OF STEEL

SUPERSTRUCTURE									
BAR	LOCATION	SIZE	NO. REQ'D	BENDING DIMENSIONS				LENGTH	
				A	B	C	D		
A401	ENDWALL A1	4	5					52'-6"	
A402	ENDWALL A1	4	5					38'-2"	
A403	RDWY. BRKT.	4	1					50'-8"	
A404	RDWY. BRKT.	4	1					36'-1"	
A405	ENDWALL A2	4	5					54'-0"	
A406	ENDWALL A2	4	5					38'-4"	
A407	RDWY. BRKT.	4	1					51'-6"	
A408	RDWY. BRKT.	4	1					38'-0"	
H500	RDWY. BRKT.	5	131	3'-9"	6"			4'-9"	
HA400	ENDWALLS	4	10	2'-9"	2'-9"	6"		8'-4"	
HB400	ENDWALLS	4	10	2'-9"	7'-1"	6'-6"		16'-3"	
L400	ENDWALLS	4	70	7'-2"	1'-0"	2'-9"		20'-10"	
YB400	ENDWALLS	4	80	2'-9"	1'-0"			4'-4"	
SUPERSTRUCTURE EPOXY									
A400E	SIDEWALK	4	662					2'-9"	
A401E	ENDWALL A1	4	1					52'-6"	
A402E	ENDWALL A1	4	1					38'-2"	
A405E	ENDWALL A2	4	1					54'-0"	
A406E	ENDWALL A2	4	1					38'-4"	
A600E	SLAB	6	1040					30'-0"	
A601E	SLAB	6	331					34'-4"	
A602E	SLAB	6	331					35'-10"	
A603E	SLAB	6	662					9'-3"	
A604E	SLAB	6	331					43'-6"	
A605E	SLAB	6	331					42'-9"	
CD600E	SIDEWALK	6	331	4'-4"	2'-8 1/4"	2'-7"		7'-0"	
CD601E	SIDEWALK	6	331	4'-4"	2'-7 1/2"	2'-7"		7'-0"	
CU600E	SLAB	6	331					6'-6"	
CU601E	SLAB	6	331					5'-0"	
F600E	SIDEWALK	6	340	2'-6"	1"	5'-8"	8"	8'-8"	
GB600E	RDWY. BRKT.	6	168	2'-9"	2'-2 1/2"	3'-1 1/2"		6'-7"	
R600E	SLAB	6	124	3'-4"	2'-5"			5'-9"	

ABUTMENT NO. 1									
BAR	LOCATION	SIZE	NO. REQ'D	BENDING DIMENSIONS				LENGTH	
				A	B	C	D		
SERIES									
A640	LEFT WING	6	2					306'-8"	
SERIES									
A641	LEFT WING	6	1					188'-9"	
A642	LEFT WING	6	5					31'-7"	
SERIES									
A643	LEFT WING	6	1					76'-3"	
A644	LEFT WING	6	4					26'-6"	
SERIES									
A645	RIGHT WING	6	2					222'-3"	
SERIES									
A646	RIGHT WING	6	1					124'-7"	
A647	RIGHT WING	6	5					26'-7"	
SERIES									
A648	RIGHT WING	6	1					47'-6"	
A649	RIGHT WING	6	4					21'-6"	
A650	LEFT WING	6	2					32'-0"	
A651	RIGHT WING	6	2					26'-6"	
C640	WINGS	6	12	4'-4"				5'-4"	
CD640	LEFT WING	6	17	5'-9"	2'-9"	6"		8'-6"	
CD641	LEFT WING	6	1	5'-3"	2'-9"	6"		8'-0"	
CD642	LEFT WING	6	27	8'-3"	2'-9"	6"		11'-0"	

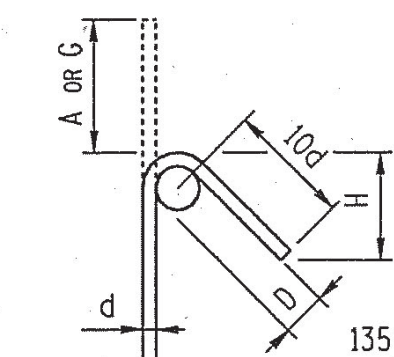
ABUTMENT NO. 1 (CONT.)									
BAR	LOCATION	SIZE	NO. REQ'D	BENDING DIMENSIONS				LENGTH	
				A	B	C	D		
SERIES									
CD643	LEFT WING	6	1		2'-9"	6"		49'-0"	
CD644	RIGHT WING	6	22	8'-4"	2'-9"			11'-1"	
SERIES									
CD645	RIGHT WING	6	1		2'-9"			50'-9"	
D640	WING FTNG.	6	12	2'-6"				3'-10"	
H440	ABUTMENT	4	13	2'-9"	3'-6"			9'-9"	
L440	ABUTMENT	4	78	15'-2"	1'-0"	2'-9"		36'-10"	
LB440	ABUTMENT	4	13	11'-9"	6"	2'-9"	11'-3"	29'-6"	
LB441	ABUTMENT	4	13	8'-3"	6"	2'-9"	7'-9"	22'-6"	
LB442	ABUTMENT	4	13	9'-11"	6"	2'-9"	9'-6"	26'-0"	
LB443	ABUTMENT	4	13	10'-3"	6"	2'-9"	9'-9"	26'-6"	
ABUTMENT NO. 1 EPOXY									
SERIES									
A1140E	ABUT./ENDW.	11	2					106'-1"	
SERIES									
A1141E	ABUT./ENDW.	11	2					773'-1"	
SERIES									
A1142E	ABUT./ENDW.	11	2					507'-4"	
SERIES									
A1143E	ABUT./ENDW.	11	2					85'-9"	
A1144E	ABUT./ENDW.	11	3					17'-9"	
A1145E	ABUT./ENDW.	11	2					14'-5"	

ABUTMENT NO. 2									
BAR	LOCATION	SIZE	NO. REQ'D	BENDING DIMENSIONS				LENGTH	
				A	B	C	D		
SERIES									
A652	RIGHT WING	6	2					411'-0"	
SERIES									
A653	RIGHT WING	6	1					122'-0"	
A654	RIGHT WING	6	2					38'-0"	
SERIES									
A655	RIGHT WING	6	1					262'-6"	
A656	LEFT WING	6	2					31'-6"	
SERIES									
A657	LEFT WING	6	2					309'-4"	
SERIES									
A658	LEFT WING	6	1					219'-5"	
A659	RIGHT WING	6	3					28'-5"	
SERIES									
A660	LEFT WING	6	1					85'-5"	
A661	RIGHT WING	6	4					37'-3"	
A662	LEFT WING	6	4					26'-5"	
A663	LEFT WING	6	4					28'-3"	
C640	WINGS	6	12	4'-4"				5'-4"	
CB642	RIGHT WING	6	15	5'-4"	2'-9"	6 1/4"		8'-1"	
CD643	LEFT WING	6	13	5'-7"	2'-9"	6 1/4"		8'-4"	
CD646	LEFT WING	6	28	8'-4"	2'-9"	9 1/2"		11'-1"	
CD647	LEFT WING	6	4	6'-0"	2'-9"	9 1/2"		8'-9"	
CD648	RIGHT WING	6	34	8'-4"	2'-9"	9 1/8"		11'-1"	

CONST. NO. 19008-3165-44

PROJECT NO.	YEAR	SHEET NO.
IR-40-5(106)221	1990	

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



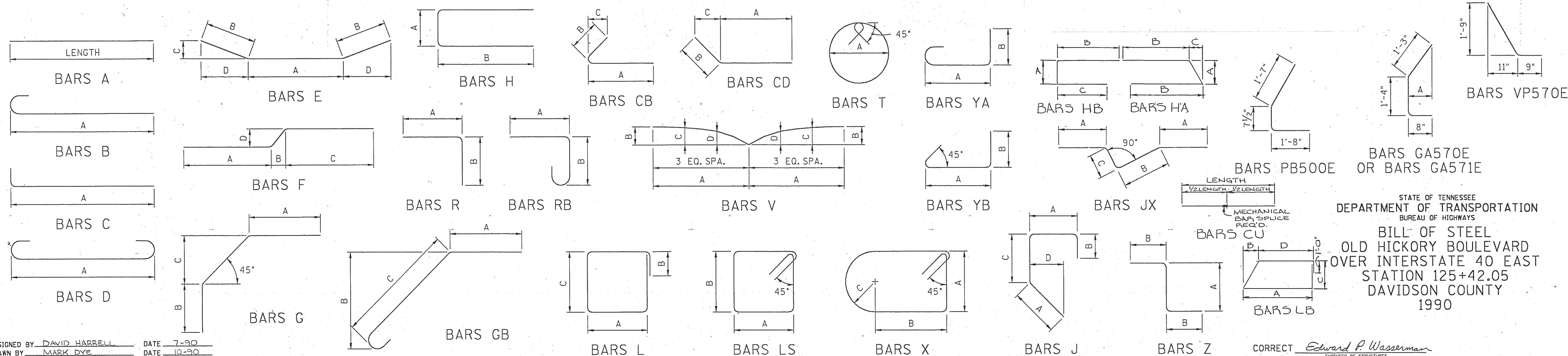
BAR SIZE	D (IN.)	135° HOOK A OR G	H APPROX.
#3	1 1/2	5	3 1/2
#4	2	6 1/2	4 1/2
#5	2 1/2	8	5 1/2
#6	4 1/2	10 1/4	6 1/2
#7	5 1/4	1-0 1/4	7 1/4
#8	6	1-2 1/4	9

135° SEISMIC STIRRUP/TIE HOOK DIMENSIONS (ALL GRADES)

REINFORCING STEEL CODE

TYPE	SIZE	SERIES
A	5	06

NOTE: DIMENSIONS SHOWN ON THIS SHEET ARE OUTSIDE TO OUTSIDE OF BAR. STANDARD C.R.S.I. HOOK DETAILS SHALL APPLY, EXCEPT AS NOTED.
NOTE: THE SUFFIX E, FOR BARS SO MARKED, DENOTES EPOXY COATED REINFORCEMENT.



DESIGNED BY: DAVID HARRELL DATE: 7-90
DRAWN BY: MARK DYE DATE: 10-90
SUPERVISED BY: BROOKS/HARRISON DATE: 10-90
CHECKED BY: DAVID HARRELL DATE: 10-90

CORRECT: Edward P. Wasserman
ENGINEER OF STRUCTURES

M-246-86

